



SAIN ASSOCIATES
122 SOUTH FIRST STREET
PULASKI, TN
MARK RANDALL, P.E. NO. 115968

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
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SAIN ASSOCIATES
122 SOUTH FIRST STREET
PULASKI, TN
JEFFREY D. STEPHENSON, P.E. NO. 108626

SHEET NAME	SHEET NO.
SIGNATURE SHEET.....	ROADWAY-SIGN1
SIGNAL PLANS	9 – 10

YEAR	PROJECT NO.	SHEET NO.
2025	HSIP-43(47)	ROADWAY-SIGN1
<p align="center">STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION</p>		
<p align="center">SIGNATURE SHEET</p>		

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

GIBSON COUNTY

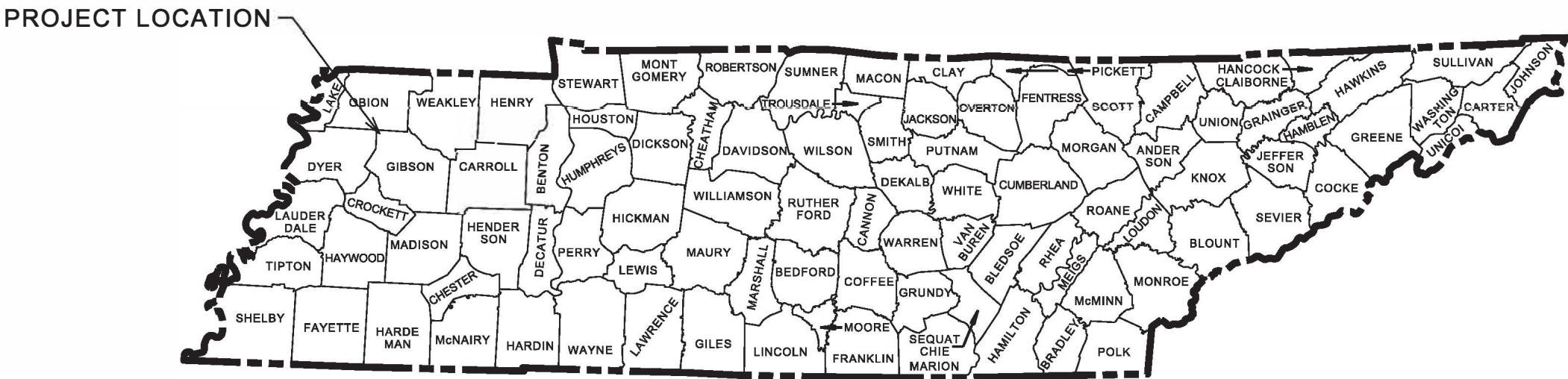
S.R. 43 (U.S. 45E, SOUTH FIRST STREET), INTERSECTION AT
KEFAUVER DR., LM 8.70 IN MILAN

PS&E
SIGNALIZATION

STATE HIGHWAY NO. 43 F.A.H.S. NO. 45E

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	HSIP-43(47)	
STATE PROJ. NO.	27004-3284-94	



NO EXCLUSIONS

27004-3284-94
END PROJECT NO. HSIP-43(47) CONSTRUCTION
STA. 457+60.02
N 584474.6250 E 1153581.5023

27004-2284-94
END PROJECT NO. HSIP-43(47) R.O.W.
STA. 454+12.00 S.R. 43
N: 584128.0485 E: 1153613.1364

27004-2284-94
BEGIN PROJECT NO. HSIP-43(47) R.O.W.
STA. 450+25.10 S.R. 43
N: 583742.8249 E: 1153649.0580

27004-3284-94
BEGIN PROJECT NO. HSIP-43(47) CONSTRUCTION
STA. 448+42.79
N 583561.2849 E 1153665.7987

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

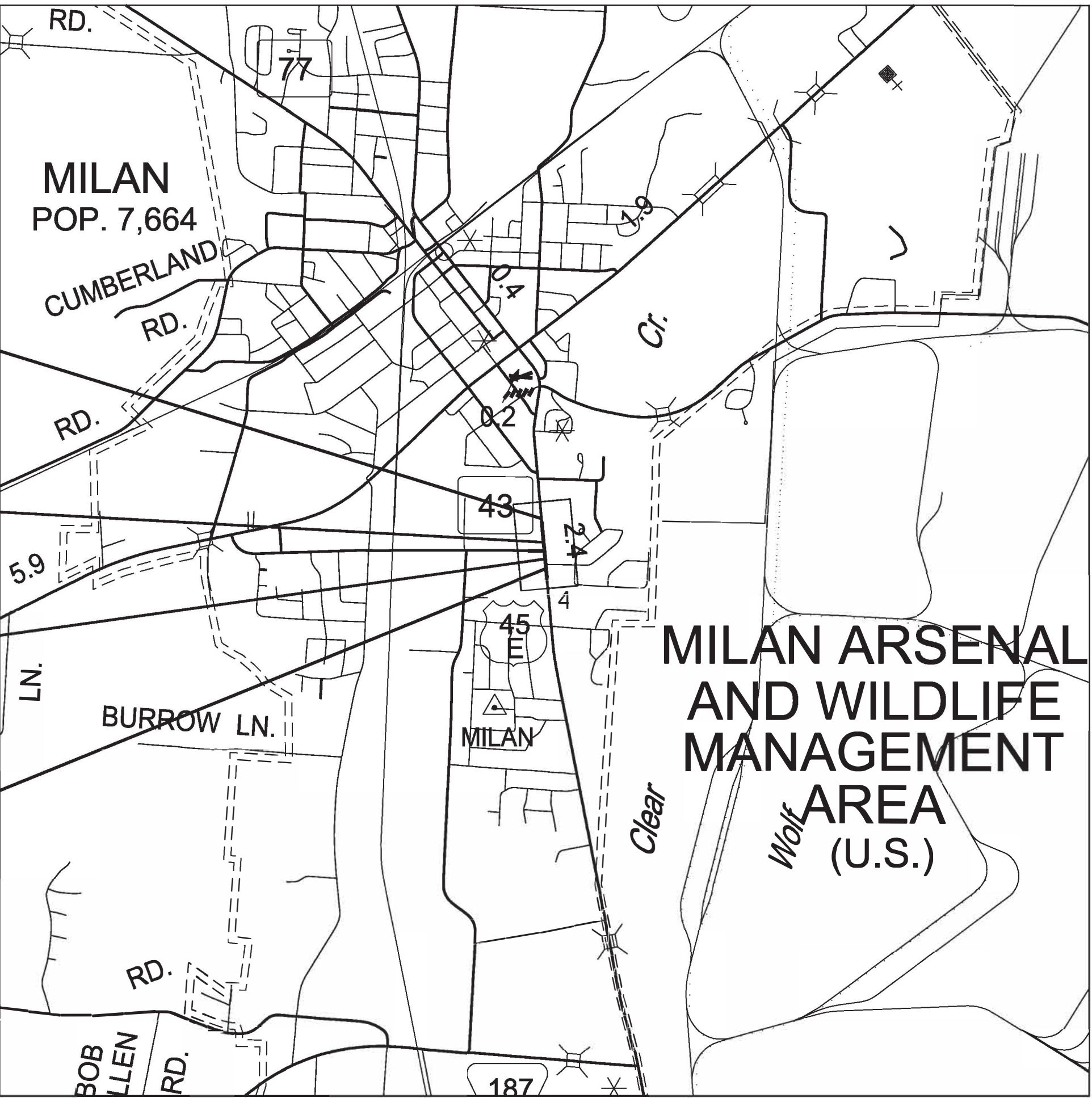
TDOT STATEWIDE TRANSPORTATION ENGINEER: STEVE BRYAN, P.E., P.T.O.E.

DESIGNED BY : SAIN ASSOCIATES

DESIGNER : MARK RANDALL, P.E. & BRANDON KENNEDY CHECKED BY RICHARD HOLT, P.E. & CHARLES COCHRAN, P.E.

P.E. NO. 27004-1284-94 (DESIGN)

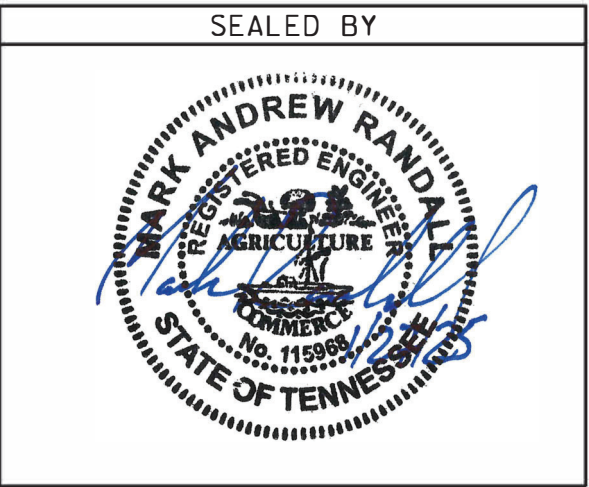
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SCALE: 1"= 2,000'

R.O.W. LENGTH	0.073 MILES
ROADWAY LENGTH	0.173 MILES
BRIDGE LENGTH	0.000 MILES
BOX BRIDGE LENGTH	0.000 MILES
BOX BRIDGE LENGTH	0.000 MILES ▲
PROJECT LENGTH	0.173 MILES

▲ Not included in the project length (Non Riding Surface).



APPROVED: WILL REID, CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, COMMISSIONER

SURVEY	08/04/2020	TRAFFIC DATA
10/20/2020 UPDATED	ADT (2025)	21,780
	ADT (2045)	24,750
	DHV (2045)	2,626
	D	55 - 45
	T (ADT)	6 %
	T (DHV)	4 %
	V	30 MPH

COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: DIVISION ADMINISTRATOR DATE

PS&E INDEX OF SHEETS

STANDARD ROADWAY DRAWINGS

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX AND STANDARD ROADWAY DRAWINGS	1A
STANDARD TRAFFIC OPERATIONS AND STRUCTURE DRAWINGS	1A1
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SIGNAL PLANS	9 – 10
TRAFFIC CONTROL PLANS	T1 – T5
UTILITIES PLANS.....	U1–1
NOTE: THE ALPHABETICAL LETTERS “I”, “O” & “Q” ARE NOT USED IN THE NUMBERING OF SHEETS.	
OMIT SHEETS 2A, 2F, & 2G	
NO PROJECT COMMITMENTS SHEET INCLUDED IN THIS SET OF PLANS	

DWG.	REV.	DESCRIPTION
STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-TP-1	10-01-24	STANDARD ROADWAY DRAWINGS TITLE SHEET
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
RD-L-3	03-01-23	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING
RD-L-4	10-01-24	STANDARD LEGEND FOR SIGNALIZATION AND LIGHTING
RD-L-5	07-05-24	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-6	02-20-20	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
RD-L-7	02-20-20	STANDARD LEGEND FOR EROSION PREVENTION AND SEDIMENT CONTROL
ROADWAY DESIGN STANDARDS		
RD11-TS-6A		TYPICAL CURB & GUTTER SECTIONS WITHOUT SHOULDERS AND WITH GRASS STRIPS
ROADWAY, PAVEMENT APPURTENANCES, AND FENCES		
RP-VC-11	03-04-21	VERTICAL CONCRETE CURB AND CURB AND GUTTER (FOR 6” & 7” GUTTER DEPTH)
S-RP-2	06-28-19	STANDARD CONCRETE RIGHT-OF-WAY MARKERS
MULTIMODAL		
MM-CR-1	06-28-19	DETECTABLE WARNING SURFACE PLACEMENT ON CURB RAMPS
MM-CR-3		PARALLEL CURB RAMP
MM-CR-5	06-28-19	SINGLE CROSSING CURB RAMP IN CURVE
MM-CR-6		DUAL CROSSING CURB RAMP PLACED OUTSIDE CURVE
MM-CR-8		MONO-DIRECTIONAL SINGLE CROSSWALK CURB RAMP DETAILS
MM-SW-1	07-07-23	DETAILS FOR CONCRETE SIDEWALKS
DESIGN - TRAFFIC CONTROL		
T-M-1	06-28-19	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-09-24	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-3	07-07-23	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	07-17-20	STANDARD INTERSECTION PAVEMENT MARKINGS
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-40	03-05-17	RIGHT LANE CLOSURES AT NEAR SIDE OF INTERSECTIONS
T-WZ-FAB1		FLASHING YELLOW ARROW BOARD
EROSION PREVENTION AND SEDIMENT CONTROL		
EC-STR-37	06-10-14	SEDIMENT TUBE
EC-STR-39	08-01-12	CURB INLET PROTECTION TYPE 1 & 2

TYPE	YEAR	PROJECT NO.	SHEET NO.
PIH	2024	HSIP-43(47)	1A
PS&E	2025	HSIP-43(47)	1A

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX
AND
STANDARD
ROADWAY
DRAWINGS

STANDARD TRAFFIC OPERATIONS DRAWINGS

DWG.	REV.	DESCRIPTION
SIGNS		
T-S-9	06-10-14	STANDARD LAYOUT GROUND MOUNTED SIGNS
T-S-10	04-04-12	STANDARD MOUNTING DETAILS FLAT SHEET SIGNS ALUMINUM-STEEL DESIGN
T-S-12	07-10-17	STANDARD STEEL GROUND MOUNTED SIGNS, BREAK- AWAY TYPE POST FOOTING DETAILS, SQUARE TUBES
T-S-13	10-21-19	STANDARD STEEL GROUND MOUNTED SIGNS, BREAK- AWAY TYPE POST FOOTING DETAILS, I-BEAMS
T-S-14	10-21-19	STANDARD STEEL GROUND MOUNTED SIGNS, BREAK- AWAY TYPE POST FOOTING DETAILS, WF-BEAMS
T-S-16	07-02-15	GROUND MOUNTED ROADSIDE SIGN PLACEMENT DETAILS
T-S-16A	07-02-15	GROUND MOUNTED ROADSIDE SIGN PLACEMENT DETAILS
T-S-17	07-11-17	STANDARD GROUND MOUNTED SIGN USING PERFORATED/KNOCKOUT SQUARE TUBE
T-S-19	06-12-20	STANDARD STEEL SIGN SUPPORTS
T-S-20	07-11-17	SIGN DETAILS
T-S-23A	07-11-17	MULTI-DIRECTIONAL SLIP BASE BREAKAWAY P-POST SIGN SUPPORT
T-S-23B	07-19-13	MULTI-DIRECTIONAL SLIP BASE BREAKAWAY STRUCTURAL PIPE SIGN SUPPORT
T-S-23C	07-02-15	BREAKAWAY POST SIGN SUPPORTS

SIGNALS

T-SG-2	06-27-16	LOOP LEAD-INS, CONDUIT AND PULL BOXES
T-SG-3A	06-27-16	ALTERNATE DETECTION DETAILS
T-SG-5	06-27-16	CONTROLLER CABINET DETAILS
T-SG-6	10-21-19	PEDESTRIAN SIGNAL DETAILS
T-SG-7	10-21-19	SIGNAL HEAD ASSEMBLIES
T-SG-7D	09-12-23	TYPICAL SIGNAL HEAD PLACEMENT TWO-LANE APPROACHES
T-SG-7F	07-13-17	TYPICAL SIGNAL HEAD PLACEMENT THREE-LANE APPROACHES
T-SG-7H	10-21-19	TYPICAL SIGNAL HEAD PLACEMENT THREE-LANE AND FOUR-LANE APPROACHES
T-SG-7L	10-21-19	TYPICAL SIGNAL HEAD PLACEMENT FOUR-LANE APPROACHES
T-SG-9	07-15-24	DETAILS OF CANTILEVER SIGNAL SUPPORT
T-SG-9A	07-12-17	MISCELLANEOUS SIGNAL DETAILS
T-SG-10	09-12-23	MAST ARM POLE AND STRAIN POLES FOUNDATION DETAILS
T-SG-11	07-12-17	MAINTENANCE OF EXISTING SIGNALS DURING HIGHWAY CONSTRUCTION
T-SG-12	12-20-19	TYPICAL WIRING FOR SIGNAL HEADS AND DETECTION LOOPS

STANDARD STRUCTURE DRAWINGS

NEW STRUCTURES

STD-8-4	SIGN, LUMINAIRE, AND TRAFFIC SIGNAL SUPPORTS
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TYPE	YEAR	PROJECT NO.	SHEET NO.
PIH	2024	HSIP-43(47)	1A1
PS&E	2025	HSIP-43(47)	1A1

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

STANDARD TRAFFIC
OPERATION AND STRUCTURE
DRAWINGS


ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 27004-3284-94
105-01	CONSTRUCTION STAKES, LINES AND GRADES	LS	1
202-03	REMOVAL OF RIGID PAVEMENT, SIDEWALK, ETC.	S.Y.	175
202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	90
202-08.10	REMOVAL OF CURB (CONCRETE)	L.F.	10
203-06	WATER	M.G.	1
203-10	EMBANKMENT (COMPACTED IN PLACE)	C.Y.	50
(1) 209-05	SEDIMENT REMOVAL	C.Y.	5
(1) (2) 209-09.41	CURB INLET PROTECTION (TYPE 2)	EACH	6
407-20.05	SAW CUTTING ASPHALT PAVEMENT	L.F.	385
701-01.01	CONCRETE SIDEWALK (4 ")	S.F.	1910
701-02.03	CONCRETE CURB RAMP	S.F.	615
702-01	CONCRETE CURB	C.Y.	5
702-03	CONCRETE COMBINED CURB & GUTTER	C.Y.	25
708-02.01	MARKERS (CONCRETE R.O.W. POSTS)	EACH	6
712-01	TRAFFIC CONTROL	LS	1
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	50
712-06	SIGNS (CONSTRUCTION)	S.F.	200
712-08.03	ARROWBOARD (TYPE C)	EACH	1
713-02.21	SIGN POST DELINEATION ENHANCEMENT	L.F.	24
(3) 713-14.21	STREET NAME SIGN (RIGID 0.100IN THICK)	S.F.	64
(4) 713-15	REMOVAL OF SIGNS, POSTS AND FOOTINGS	LS	1
713-15.07	SUSPENDED FLAT SHEET ALUMINUMSIGN (0.080" THICK)	EACH	2
713-16.01	CHANGEABLE MESSAGE SIGN UNIT	EACH	3
713-16.21	SIGNS (W3-3, 36"x36")	EACH	3
713-16.22	SIGNS (R1-1, 36"x36")	EACH	1
713-16.23	SIGNS (R2-1, 24"x30")	EACH	1
713-16.24	SIGNS (R10-7, 24"x30")	EACH	1
714-05.04	PULL BOXES (TYPE C)	EACH	2
716-01.21	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH	47
716-01.22	SNOWPLOWABLE RAISED PAVMENT MARKERS (MONO-DIR)(1 COLOR)	EACH	52
(5) 716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	125
(5) 716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	180
(5) 716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	8
(5) 716-02.08	PLASTIC PAVEMENT MARKING (8" DOTTED LINE)	L.F.	185
(5) 716-02.09	PLASTIC PAVEMENT MARKING (LONGITUDINAL CROSS-WALK)	L.F.	240
(5) 716-02.12	PLASTIC PAVEMENT MARKING (8IN LINE)	L.M.	1
(5) 716-03.01	PLASTIC WORD PAVEMENT MARKING (ONLY)	EACH	2
(5) 716-03.09	PLASTIC WORD PAVEMENT MARKING (DO NOT BLOCK)	EACH	1
(5) 716-04.01	PLASTIC PAVEMENT MARKING (STRAIGHT-TURN ARROW)	EACH	1
(5) 716-04.04	PLASTIC PAVEMENT MARKING (TRANSVERSE SHOULDER)	L.F.	105
716-08.02	REMOVAL OF PAVEMENT MARKING (8" BARRIER LINE)	L.F.	355
716-08.03	REMOVAL OF PAVEMENT MARKING (CROSS-WALK)	L.F.	430
716-08.04	REMOVAL OF PAVEMENT MARKING (CHANNELIZATION STRIPING)	S.Y.	25
716-08.05	REMOVAL OF PAVEMENT MARKING (STOP LINE)	L.F.	135
716-08.06	REMOVAL OF PAVEMENT MARKING (TURN LANE ARROW)	EACH	9
716-08.07	REMOVAL OF PAVEMENT MARKING (STRAIGHT-TURN ARROW)	EACH	1
716-08.09	REMOVAL OF PAVEMENT MARKING (DOTTED LINE)	L.F.	110
716-08.10	REMOVAL OF PAVEMENT MARKING (TRANSVERSE SHOULDER)	L.F.	105
716-08.20	REMOVAL OF PAVEMENT MARKING (LINE)	L.M.	1
716-12.02	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	1
716-13.02	SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE)	L.M.	1
717-01	MOBILIZATION	LS	1
(6) 730-01.02	REMOVAL OF SIGNAL EQUIPMENT	EACH	1
730-02.03	SIGNAL HEAD ASSEMBLY (130 A3 WITH BACKPLATE)	EACH	2
730-02.09	SIGNAL HEAD ASSEMBLY (130 WITH BACKPLATE)	EACH	6
730-02.14	SIGNAL HEAD ASSEMBLY (140 A1 WITH BACKPLATE)	EACH	1
730-02.59	SIGNAL HEAD ASSEMBLY (140 A4F WITH BACKPLATE)	EACH	4
730-03.21	INSTALL PULL BOX (TYPE B)	EACH	4
(7) 730-05.01	ELECTRICAL SERVICE CONNECTION	EACH	1
730-08.02	SIGNAL CABLE - 5 CONDUCTOR	L.F.	2500
730-08.03	SIGNAL CABLE - 7 CONDUCTOR	L.F.	1650

ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 27004-3284-94
730-12.02	CONDUIT 2" DIAMETER (PVC SCHEDULE 40)	L.F.	220
730-12.03	CONDUIT 3" DIAMETER (PVC SCHEDULE 40)	L.F.	50
730-12.08	CONDUIT 2" DIAMETER (RGS)	L.F.	20
730-12.24	CONDUIT 3" DIAMETER (DIRECTIONAL BORE)	L.F.	350
730-13.07	VEHICLE DETECTOR (SIREN ACTIVATED PRIORITY CONTROL)	EACH	3
(8) 730-13.14	VEHICLE DETECTOR (360-DEGREE CAMERA)	EACH	1
730-15.32	CABINET (EIGHT PHASE BASE MOUNTED)	EACH	1
(9) 730-16.04	CONTROLLER (ATC)	EACH	1
(10) 730-23.28	PEDESTAL POLE (TYPE A)	EACH	3
(11) 730-25.05	CANTILEVER SIGNAL SUPPORT (2 MASTARMS @ 50' & 65')	EACH	1
(11) 730-25.13	CANTILEVER SIGNAL SUPPORT (1 MASTARM @ 65')	EACH	1
(11) 730-25.16	CANTILEVER SIGNAL SUPPORT (1 MASTARM @ 80')	EACH	1
730-26.06	PEDESTRIAN PUSHBUTTON POST	EACH	1
730-26.09	PEDESTRIAN PUSHBUTTON WITH 15IN SIGN	EACH	1
730-26.11	COUNTDOWN PED SGNL HEAD WAUDIBLE PUSH BUTTON & 15IN SIGN	EACH	5
730-35.06	BATTERY BACK-UP SYSTEM	EACH	1
730-40	TEMPORARY TRAFFIC SIGNAL SYSTEM	EACH	1
(1) (2) 740-11.02	TEMPORARY SEDIMENT TUBE 12IN	L.F.	690
797-11.65	REMOVAL OF CONCRETE CURB & GUTTER	L.F.	385
801-03	WATER (SEEDING & SODDING)	M.G.	1
803-01	SODDING (NEWSOD)	S.Y.	25

FOOTNOTES	
(1)	SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE AND REPLACEMENT.
(2)	TO BE USED AS DIRECTED BY ENGINEER.
(3)	STREET NAME SIGNS TO BE RETROREFLECTIVE GREEN COLOR, TO CURRENT MUTCD REQUIREMENTS.
(4)	SEE SHEET 4 FOR SIGNS TO BE REMOVED.
(5)	THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC, PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
(6)	SEE TRAFFIC SIGNAL LAYOUT REMOVAL DIAGRAM FOR EQUIPMENT TO BE REMOVED.
(7)	THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE LOCAL UTILITY TO OBTAIN THE ESTIMATE FOR ANY CHARGES BY THE UTILITY FOR PROVIDING ELECTRICAL SERVICE TO THE SIGNAL CONTROLLER. THESE CHARGES AND ANY OTHER EQUIPMENT NECESSARY FOR A COMPLETE SERVICE CONNECTION SHALL BE INCLUDED IN THE PRICE BID FOR THIS ITEM FOR PAYMENT BY THE CONTRACTOR.
(8)	VEHICLE DETECTION SHALL BE MIOVISION SMART VIEW 360 CAMERA VEHICLE DETECTION SYSTEM (PROPRIETARY ITEM)
(9)	THE TRAFFIC CONTROLLER FURNISHED BY THE CONTRACTOR SHALL BE SIEMENS (PROPRIETARY ITEM) AND COMPLETE WITH ALL INCIDENTAL AND AUXILIARY EQUIPMENT (CONFLICT MONITOR AND HARDWARE) NECESSARY FOR INSTALLATION AND OPERATION EITHER AS A REMOTE LOCATION OR AS PART OF A SYSTEM OF INTERSECTIONS. ALL WIRING AND EQUIPMENT NECESSARY TO ACTIVATE THE SIGNAL HEADS AND OPERATE THE TRAFFIC SIGNAL AS SPECIFIED SHALL BE PROVIDED AND INSTALLED BY THE CONTRACTOR.
(10)	THE PEDESTAL POLES (TYPE A) SHALL HAVE A HEIGHT OF 10'.
(11)	SEE SPECIAL PROVISION 700SIG FOR POLE DESIGN REQUIREMENTS. BID ITEM SHALL INCLUDE THE COST OF ALL MATERIALS AND LABOR NECESSARY FOR COMPLETE INSTALLATION OF THE POLE FOUNDATION. SELECT THE APPROPRIATE FOUNDATION DESIGN FROM STANDARD DRAWING T-SG-10.

TYPE	YEAR	PROJECT NO.	SHEET NO.
PIH	2024	HSIP-43(47)	2
PS&E	2025	HSIP-43(47)	2

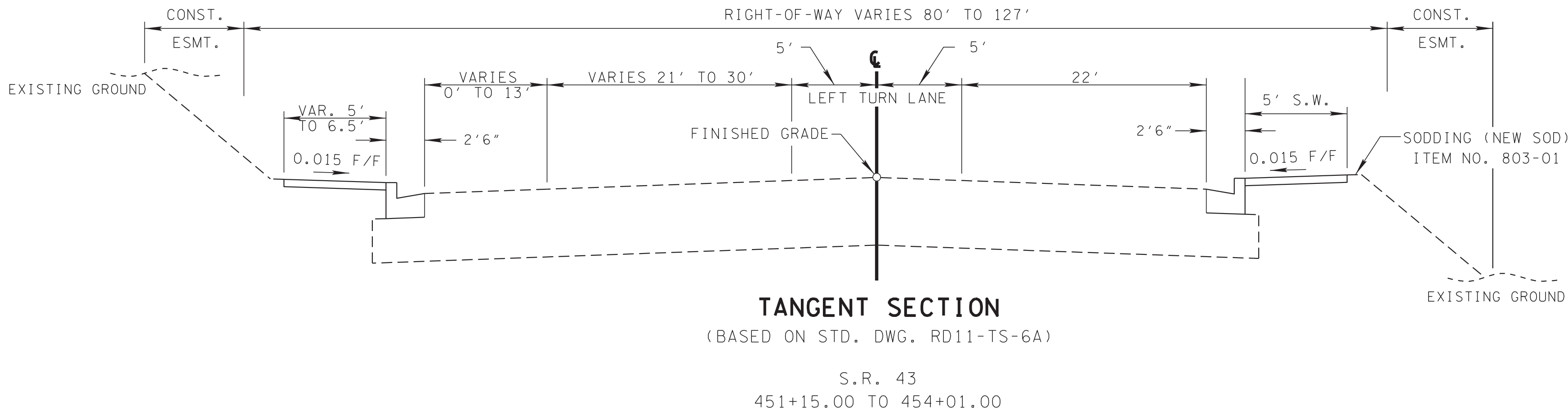
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	2B
PIH	2024	HSIP-43(47)	2B
PS&E	2025	HSIP-43(47)	2B



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DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (2) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

SEEDING AND SODDING

- (1) SOD SHALL BE PLACED AT LOCATIONS SHOWN ON THE PLANS TO PREVENT DAMAGE TO ADJACENT FACILITIES AND PROPERTY DUE TO EROSION ON ALL NEWLY GRADED CUT AND FILL SLOPES AS WORK PROGRESSES.

MISCELLANEOUS

- (1) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (2) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

FINAL PAVEMENT MARKING

- (1) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.
- (2) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6” SPRAY THERMOPLASTIC (60 mil) INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-13.02, SPRAY THERMO PVMT MRKNG (60 mil) (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SIGNING

- (1) FOR ALL PERMANENT PANEL SIGNS WITH A SILVER-WHITE, YELLOW, RED, GREEN, BROWN, OR BLUE BACKGROUND, PROVIDE REFLECTIVE SHEETING THAT MEETS OR EXCEEDS AASHTO M268, TYPE D.
- (2) THE TOP OF THE SIGN FOOTINGS SHALL BE PLACED LEVEL WITH THE GROUND LINE.
- (3) AFTER THE SIGN LOCATIONS HAVE BEEN STAKED, BUT PRIOR TO ORDERING ANY MATERIAL FOR THE SUPPORTS, THERE SHALL BE A FIELD INSPECTION AND APPROVAL BY THE REGIONAL CONSTRUCTION OFFICE.
- (4) THE CONTRACTOR SHALL BE REQUIRED TO FURNISH LAYOUT DRAWINGS OF ALL EXTRUDED PANEL SIGNS WITH SPACING OF ALL LETTERS, NUMERALS, SHIELDS, AND ARROWS. ONE PDF SET OF THE LAYOUT DRAWINGS SHALL BE SENT TO THE TRAFFIC OPERATIONS DIVISION, SIGNING SECTION (TDOT.TrafficOps.Sign-Reviews@tn.gov) FOR REVIEW. ONE PDF SET OF THE LAYOUT DRAWINGS SHALL BE SENT TO THE REGIONAL SIGN DESIGNER FOR REVIEW.

- (5) ALL SIGNS MARKED “TO BE REMOVED” ARE TO BE REMOVED BY THE CONTRACTOR AND PAID FOR UNDER ITEM NO. 713-15 AND BECOME THE PROPERTY OF THE CONTRACTOR.
- (6) THE EXISTING FOOTINGS ARE TO BE REMOVED 6 INCHES BELOW GROUND LINE.
- (7) THE LETTERS, DIGITS, ARROWS, BORDERS, AND ALPHABET ACCESSORIES ON ALL FLAT SHEET SIGNS SHALL BE APPLIED BY SILK SCREENING PROCESS, EXCEPT THAT CUT-OUT DIRECT APPLIED COPY SHALL BE USED ON ALL FLAT SHEET SIGNS WITH A GREEN BACKGROUND, OR BROWN BACKGROUND.
- (8) THE LETTERS, DIGITS, ARROWS, BORDERS, AND ALPHABET ACCESSORIES ON ALL FLAT SHEET SIGNS SHALL BE APPLIED BY SILK SCREENING PROCESS.
- (9) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.


- (8) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

SIGNALIZATION

- (1) EQUIPMENT AND INSTALLATION OF TRAFFIC SIGNALS SHALL COMPLY WITH TDOT STANDARD SPECIFICATIONS, SECTION 730.
- (2) SALVAGEABLE EQUIPMENT SHALL BECOME THE PROPERTY OF THE CITY OF MILAN AND SHALL BE STOCKPILED AT A LOCATION DESIGNATED BY THE ENGINEER FOR PICKUP BY THE CITY OF MILAN.
- (3) ANY SIGNAL HEADS, WHEN VISIBLE TO DRIVERS BUT NOT OPERATIONAL, SHALL BE COMPLETELY COVERED.

TYPE	YEAR	PROJECT NO.	SHEET NO.
PIH	2024	HSIP-43(47)	2C
PS&E	2025	HSIP-43(47)	2C

SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
NOTES

SPECIAL NOTES

MULTIMODAL

- (1) DURING CONSTRUCTION, IF THE CONSTRUCTION SUPERVISOR IDENTIFIES CURB RAMP LOCATIONS WITHIN THE PROJECT LIMITS WHERE THE TDOT ROADWAY STANDARDS CANNOT BE USED DUE TO SITE LIMITATIONS, A SKETCH OR PICTURE, SHOWING EXISTING CONDITIONS AS WELL AS PROPOSED MODIFICATIONS SHOULD BE SUBMITTED TO THE REGIONAL PROJECT DEVELOPMENT OFFICE THREE WEEKS PRIOR TO THE BEGINNING OF CURB RAMP CONSTRUCTION. THE OFFICE WILL REVIEW AND EVALUATE THE LOCATIONS TO DEVELOP PROPER CURB RAMP DESIGN THAT WILL MEET REGULATIONS.

SIGNAL HEADS

- (1) ALL CIRCULAR AND ARROW INDICATIONS WITHIN ALL VEHICULAR SIGNAL HEADS PROPOSED FOR THIS PROJECT SHALL CONSIST OF AN LED (LIGHT EMITTING DIODE) SIGNAL MODULE UNLESS OTHERWISE NOTED IN THE PLANS.
- (2) CIRCULAR INDICATIONS SHALL MEET "ITE VTCSH-LED CIRCULAR SIGNAL SUPPLEMENT" FOR EXPANDED/EXTENDED VIEW. ARROW INDICATIONS SHALL MEET "ITE VTCSH-3 LED ARROW SPECIFICATON" FOR EXPANDED/EXTENDED VIEW. PEDESTRIAN INDICATIONS SHALL MEET "ITE PTCSI PART 2".
- (3) INCANDESCENT OR SCREW-IN MODULES ARE NOT ACCEPTABLE.
- (4) COMPATABILITY WITH CONFLICT MONITORS AND LOAD SWITCHES SHALL BE TESTED AND CONFIRMED.
- (5) MANUFACTURER SHALL PROVIDE A MINIMUM FIVE-YEAR WARRANTY FOR OPERATION OF THE UNIT.
- (6) ALL PEDESTRIAN TRAFFIC CONTROL INDICATIONS, WHERE CALLED FOR, SHALL CONSIST OF LED MODULES DISPLAYING "WALKING PERSON" AND "HAND" SYMBOLS, ALONG WITH A PEDESTRIAN INTERVAL COUNTDOWN DISPLAY , WITHIN THE SAME FACE UNLESS OTHERWISE NOTED IN THE PLANS
- (7) SIGNAL HEADS SHALL INCLUDE LOUVERED BACKPLATES WITH A 1" MINIMUM/ 3" MAXIMUM YELLOW RETRO REFLECTIVE BORDER AROUND THE PERIMETER OF THE FACE OF THE BACKPLATE. THE RETRO REFLECTIVE BORDER IS TO BE MADE OF A TYPE III PRISMATIC OR BETTER MATERIAL

TYPE	YEAR	PROJECT NO.	SHEET NO.
PIH	2024	HSIP-43(47)	2D
PS&E	2025	HSIP-43(47)	2D

SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES

ENVIRONMENTAL NOTES

SUBSECTION 1 – ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (1)

SOIL MATERIALS MUST BE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. EPSC MEASURES TO PROTECT NATURAL RESOURCES AND WATER QUALITY SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD. APPROPRIATE EPSC MEASURES MUST BE INSTALLED ALONG THE BASE OF ALL FILLS AND CUTS, ON THE DOWNHILL SIDE OF STOCKPILED SOIL, AND ALONG NATURAL RESOURCES IN CLEARED AREAS TO PREVENT SEDIMENT MIGRATION INTO STREAMS, WETLANDS OR OTHER NATURAL FEATURES IN ACCORDANCE WITH TDOT STANDARDS. EPSC MEASURES SHALL BE INSTALLED ON THE CONTOUR, ENTRENCHED AND STAKED, AND EXTEND THE WIDTH OF THE AREA TO BE CLEARED.
- (2)

NEW CHANNEL CONSTRUCTION SHALL BE COMPLETED IN THE DRY AND STABILIZED FOR AT LEAST 72 HOURS PRIOR TO DIVERTING WATER FROM THE EXISTING AND/OR TEMPORARY CHANNEL.
- (3)

INSTREAM EPSC DEVICES REQUIRE THE TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION REVIEW AND MUST BE PROCESSED BY THE PERMITS SECTION TO OBTAIN WATER QUALITY PERMITS.
- (4)

THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (5)

THE WIDTH OF THE FILL ASSOCIATED WITH TEMPORARY CROSSINGS SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR THE ACTUAL CROSSING, NOT TO EXCEED THE WIDTH SPECIFIED IN THE STANDARD DRAWING.
- (6)

STREAM BEDS SHALL NOT BE USED AS TRANSPORTATION ROUTES FOR CONSTRUCTION EQUIPMENT. TEMPORARY CULVERT CROSSINGS SHALL BE LIMITED TO ONE POINT PER STREAM AND EPSC MEASURES SHALL BE USED WHERE THE STREAM BANKS ARE DISTURBED. WHERE THE STREAMBED IS NOT COMPOSED OF BEDROCK, A PAD OF CLEAN ROCK SHALL BE USED AT THE CROSSING POINT AND CULVERTED TO PREVENT THE IMPOUNDMENT OF WATER FLOW. CLEAN ROCK IS ROCK OF VARIOUS TYPE AND SIZE, DEPENDING UPON APPLICATION, WHICH CONTAINS NO FINES, SOILS, OR OTHER WASTES OR CONTAMINANTS. OTHER MATERIALS USED FOR ALL TEMPORARY FILLS SHALL BE COMPLETELY REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED AND THE AFFECTED AREAS RETURNED TO PREEXISTING ELEVATIONS. ALL TEMPORARY CROSSINGS SHALL BE CONSTRUCTED IN ACCORDANCE WITH STD. DWG. EC-STR-25 UNLESS SPECIFICALLY ADDRESSED IN THE EPSC PLANS. ALTERNATIVELY, PLACING A TEMPORARY BRIDGE (E.G. BAILEY BRIDGE OR EQUIVALENT, TIMBERS, ETC.) FROM TOP OF BANK TO TOP OF BANK OR THE APPROPRIATE USE OF BARGES AT THE CROSSING TO AVOID DISTURBANCE OF THE STREAMBED IS AN ACCEPTABLE OPTION.
- (7)

HEAVY EQUIPMENT WORKING IN WETLANDS WITH PERMITTED TEMPORARY IMPACTS SHALL BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE AND COMPACTION UNLESS SPECIFICALLY ADDRESSED IN THE CONSTRUCTION PLANS. ANY MATS AND OTHER MEASURES USED FOR HEAVY EQUIPMENT SHALL BE REMOVED IN THEIR ENTIRETY AFTER THE WORK IS COMPLETED. ALL AFFECTED AREAS SHOULD BE RETURNED TO PRE-EXISTING CONDITIONS.
- (8)

WETLANDS SHALL NOT BE USED AS EQUIPMENT STORAGE, STAGING, OR TRANSPORTATION AREAS, UNLESS SPECIFICALLY PROVIDED FOR IN THE CONSTRUCTION PLANS AND PERMITS.
- (9)

THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (10)

NO ACTIVITY MAY SUBSTANTIALLY DISRUPT THE MOVEMENT OF THOSE SPECIES OF AQUATIC LIFE INDIGENOUS TO THE WATER BODY, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA.

- (11)

SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12)

IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (13)

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR AND OBTAIN ANY NECESSARY ENVIRONMENTAL PERMITS OR APPROVALS, INCLUDING BUT NOT LIMITED TO ARCHAEOLOGY, ECOLOGY, HISTORICAL, HAZARDOUS MATERIALS, AIR AND NOISE, TDEC ARAP/401, USACE SECTION 404, TVA SECTION 26A, AND TDEC NPDES PERMITS, FROM FEDERAL, STATE AND/OR LOCAL AGENCIES REGARDING ANY MATERIAL AND STAGING AREAS AND THE OPERATION OF ANY PROJECT-DEDICATED ASPHALT AND/OR CONCRETE PLANTS TO BE USED. ANY SUCH PERMITS SHALL BE SUPPLIED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO THE USE OF THE PERMITTED AREA(S).
- (14)

ANY DISAGREEMENT BETWEEN THE CONSTRUCTION PLANS, THE PROJECT AS CONSTRUCTED, AND THE PERMIT(S) ISSUED FOR THE PROJECT, SHALL BE BROUGHT TO THE ATTENTION OF THE TDOT PROJECT RESPONSIBLE PARTY. THE ENVIRONMENTAL DIVISION, DESIGN DIVISION, AND HEADQUARTERS CONSTRUCTION OFFICE SHALL BE CONTACTED IN THESE INSTANCES AND DECIDE WHICH HAS PRECEDENCE AND WHETHER PERMIT OR PLANS REVISIONS ARE NEEDED. IN GENERAL, PERMIT CONDITIONS WILL PREVAIL.
- (15)

IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.
- (16)

THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATE. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.
- (17)

ALL WATER QUALITY PERMITS SHALL BE POSTED NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE ACCESSIBLE TO THE PUBLIC. THE NAME, COMPANY NAME, EMAIL ADDRESS, TELEPHONE NUMBER AND ADDRESS OF THE PROJECT SITE OWNER, OPERATOR, OR A LOCAL CONTACT PERSON WITH A BRIEF DESCRIPTION OF THE PROJECT SHALL ALSO BE POSTED. IF POSTING THIS INFORMATION NEAR A MAIN ENTRANCE IS INFEASIBLE, THE INFORMATION SHALL BE PLACED IN A PUBLICLY ACCESSIBLE LOCATION NEAR WHERE THE CONSTRUCTION IS ACTIVELY UNDERWAY AND MOVED AS NECESSARY. THIS LOCATION SHALL BE POSTED AT THE CONSTRUCTION SITE. ALL POSTINGS SHALL BE MAINTAINED IN LEGIBLE CONDITION.

SUPPORT ACTIVITIES

- (18)

MATERIALS AND STAGING AREAS SHALL NOT AFFECT ANY WATERS OF THE STATE/U.S. UNLESS THESE AREAS ARE SPECIFICALLY COVERED BY ENVIRONMENTAL PERMITS, OBTAINED SOLELY BY THE CONTRACTOR. THE CONTRACTOR SHALL REVIEW ALL EXISTING PERMITS TO ENSURE THAT WORK AT PERMITTED SITES DOES NOT EXCEED EXPIRATION DATES. IF WORK IS GOING TO BE CONTINUED AFTER EXPIRATION DATES, THE CONTRACTOR SHALL CONTACT THE TDOT PROJECT RESPONSIBLE PARTY TO COMMENCE PERMIT RENEWAL PROCESS.

ENVIRONMENTAL

- (20)

EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

SUBSECTION 2 – ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3)

STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4)

ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

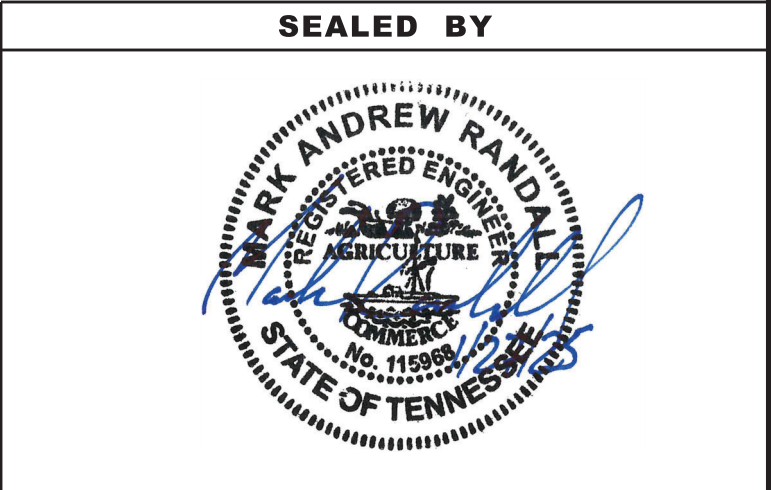
PROJECT COMMITMENTS

SCOPE OF WORK

- (6)

TRAFFIC SIGNAL INSTALLATION, SIGNING, STRIPING, AND SIDEWALKS.

TYPE	YEAR	PROJECT NO.	SHEET NO.
PIH	2024	HSIP-43(47)	2E
PS&E	2025	HSIP-43(47)	2E



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
NOTES

1/6/2025
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UTILITY

- (1) THE LOCATIONS OF UTILITIES SHOWN WITHIN THESE PLANS ARE APPROXIMATE ONLY. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. ABOVE GRADE AND UNDERGROUND UTILITIES SHOWN WERE TAKEN FROM VISIBLE APPURTENANCES AT THE SITE, PUBLIC RECORDS, AND/OR MAPS PREPARED BY OTHERS. THEREFORE, RELIANCE UPON THE TYPE, SIZE, AND LOCATION OF UTILITIES SHOWN SHOULD BE DONE SO WITH THIS CIRCUMSTANCE CONSIDERED. DETAILED VERIFICATION OF EXISTENCE, LOCATION, AND DEPTH SHOULD ALSO BE MADE PRIOR TO ANY DECISION RELATIVE THERETO IS MADE. AVAILABILITY AND COST OF SERVICE SHOULD BE CONFIRMED WITH THE APPROPRIATE UTILITY COMPANY. IN TENNESSEE, IT IS A REQUIREMENT, PER "THE UNDERGROUND UTILITY DAMAGE PREVENTION ACT", THAT ANYONE WHO ENGAGES IN EXCAVATION MUST NOTIFY ALL KNOWN UNDERGROUND UTILITY OWNERS, NO LESS THAN THREE (3) OR NOT MORE THAN TEN (10) WORKING DAYS PRIOR TO THE DATE OF THEIR INTENT TO EXCAVATE AND ALSO TO AVOID ANY POSSIBLE HAZARD OR CONFLICT. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC., AT 1-800-351-1111 AS REQUIRED BY TCA 65-31-106 WILL BE REQUIRED.
- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106 NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

CABLE:
HUGHES NET
11717 EXPLORATION LANE
GERMANTOWN, MD 20876
CONTACT: JOHN FANELLI
OFFICE PHONE: (301) 482-7124 / (301) 674-8644
Email: JOHN.FANELLI@HUGHES.NET

COMMUNICATIONS:
AT&T
315 EAST COLLEGE ST.
JACKSON, TN 38301
CONTACT: DANIEL R. POTTS
OFFICE PHONE: (901) 488-2359
Email: DP7607@ATT.COM

ELECTRIC:
MILAN DEPT. OF PUBLIC UTILITIES / WORKS
1085 S. 2ND STREET OR P.O. BOX 109
MILAN, TN 38358
CONTACT: JIM SUTCLIFFE
OFFICE PHONE: (731) 686-1537
Email: JSUTCLIFFE@MPU1.COM

GAS:
GIBSON COUNTY GAS
P.O. BOX 350
TRENTON, TN 38382
CONTACT: SAM LOCKE
OFFICE PHONE: (731) 414-2202
Email: SAM@GCUUD.NET

SANITARY SEWER:
MILAN DEPT. OF PUBLIC UTILITIES / WORKS
1085 SOUTH 2ND STREET OR P.O. BOX 109
MILAN, TN 38358
CONTACT: DAN HUMPHREY
OFFICE PHONE: (731) 414-7545
Email: DHUMPHREY@MPU1.COM

WATER:
MILAN DEPT. OF PUBLIC UTILITIES / WORKS
1085 SOUTH 2ND STREET OR P.O. BOX 109
MILAN, TN 38358
CONTACT: DAN HUMPHREY
OFFICE PHONE: (731) 414-7545
Email: DHUMPHREY@MPU1.COM

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	3
PIH	2024	HSIP-43(47)	3
PS&E	2025	HSIP-43(47)	3

REV. 04-01-24: CHANGED TRACT 6 PROPERTY OWNER, DEED BOOK & PAGE

R.O.W. MARKERS				
SHEET NO.	QUANTITIES			
	"A"	"B"	"C"	TOTALS
4A		3		3
4A			3	3
TOTALS	0	3	3	6

DISTURBED AREA		
IN BETWEEN SLOPE LINES	0.108	(AC)
TO R.O.W. OR EASEMENT (OUT SIDE SLOPE LINES)	0.168	(AC)
TOTAL DISTURBED AREA	0.276	(AC)
TOTAL PROJECT AREA	2.056	(AC)

R.O.W. ACQUISITION TABLE

TRACT NO.	PROPERTY OWNERS	COUNTY RECORDS				TOTAL AREA (ACRES)			AREA TO BE ACQUIRED (ACRES)			AREA REMAINING (ACRES)		EASEMENT (ACRES)			
		TAX MAP NO.	PARCEL NO.	DEED DOCUMENT REFERENCE		LEFT	RIGHT	TOTAL	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERMANENT	SLOPE	CONSTRUCTION (1)	AIR RIGHTS
				BOOK	PAGE												
1	CAROLYN JEAN GARMANY	140 L	2	1011	612	0.740		0.740				0.740					
2	JERRY BELEW AND WIFE LYNDA BELEW; AND TOMMY BELEW AND WIFE, JUDY BELEW	140 L	6	454	710		0.499	0.499					0.499				
3	CUMBERLAND PRESBYTERIAN CHURCH OF MILAN ,TENNESSEE	140 M	GP.A001.01	7	325	9.950		9.950				9.950				694 S.F.	
4	NEW HOPE PRIMITIVE BAPTIST CHURCH, TRUSTEES	140 L	7	1026	737		3.032	3.032					3.032				
5	LEVON EMERT AND WIFE, KATHY EMERT	140 L	GP.B028.00	571	570		1.008	1.008					1.008			103 S.F.	
6	MCDONALD'S REAL ESTATE COMPANY	140 L	GP.B027.00	1101	289		0.827	0.827		498 S.F.	498 S.F.		0.816			117 S.F.	
7	THE SHOPS OF MILAN ,A TENNESSEE GENERAL PARTNERSHIP	140 L	GP.A001.00	975	1661		6.576	6.576		1602 S.F.	1602 S.F.		6.539			2125 S.F.	
8	KROENKE-LAURIE INVESTMENT D/B/A MILAN PLAZA SHOPPING CENTER	140 F	GP.F032.03	970	1080	11.689		11.689				11.689				638 S.F.	
9	ERNEST G. SCHERER	140 L	2	1019	1225		0.549	0.549					0.549				
ACQUISITION TOTALS (ACRES)									2100 S.F.							3677 S.F.	

(1) FOR WORKING ROOM DURING CONSTRUCTION

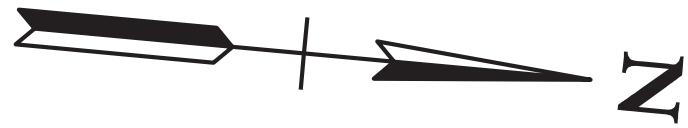
SEALED BY



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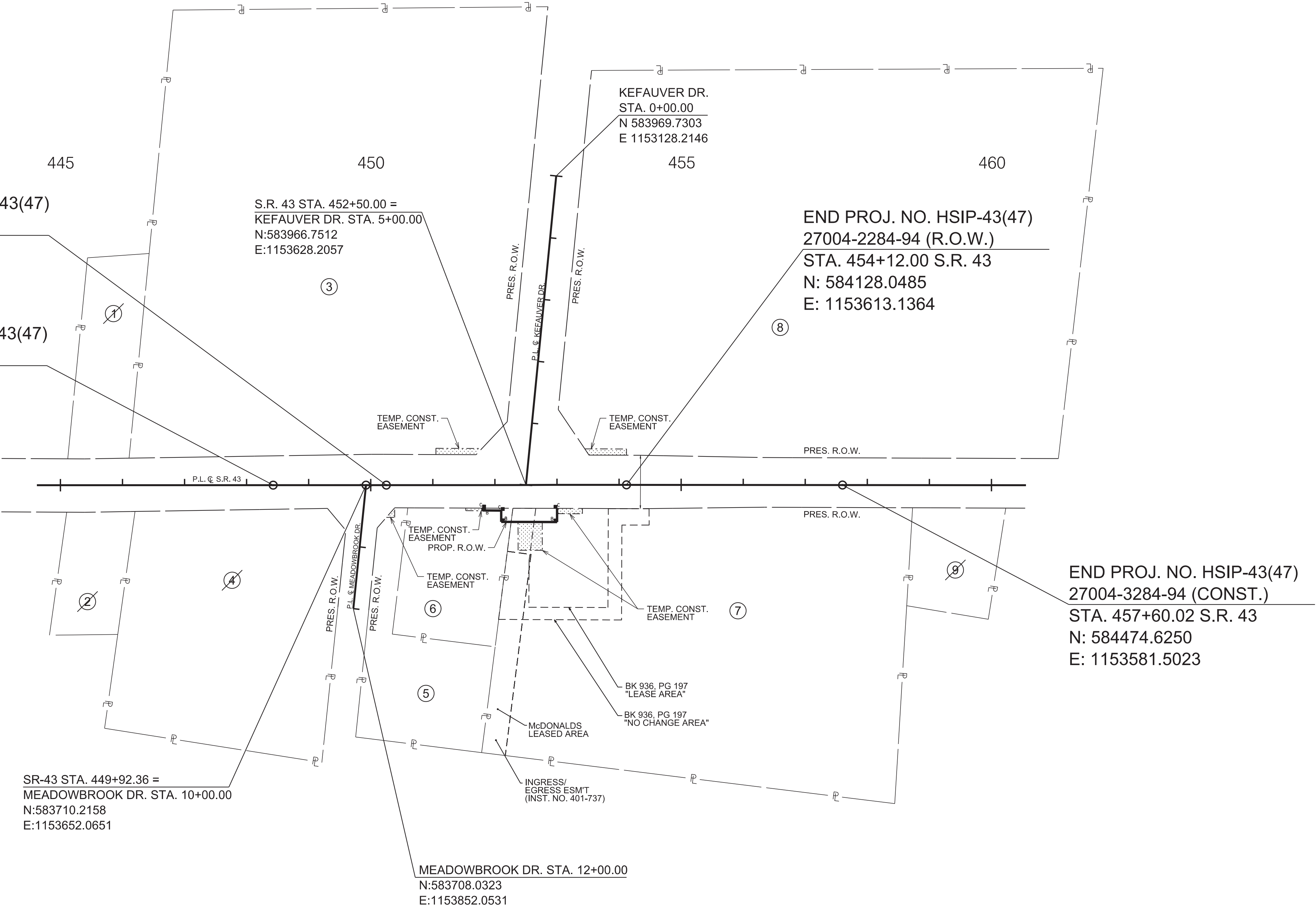
RIGHT-OF-WAY
ACQUISITION TABLE,
UTILITY NOTES, &
UTILITY OWNERS

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	3A
PIH	2024	HSIP-43(47)	3A
PS&E	2025	HSIP-43(47)	3A



BEGIN PROJ. NO. HSIP-43(47)
27004-2284-94 (R.O.W.)
STA. 450+25.10 S.R. 43
N: 583742.8249
E: 1153649.0580

BEGIN PROJ. NO. HSIP-43(47)
27004-3284-94 (CONST.)
STA. 448+42.79 S.R. 43
N: 583561.2849
E: 1153665.7987



TEMPORARY CONST. EASEMENT

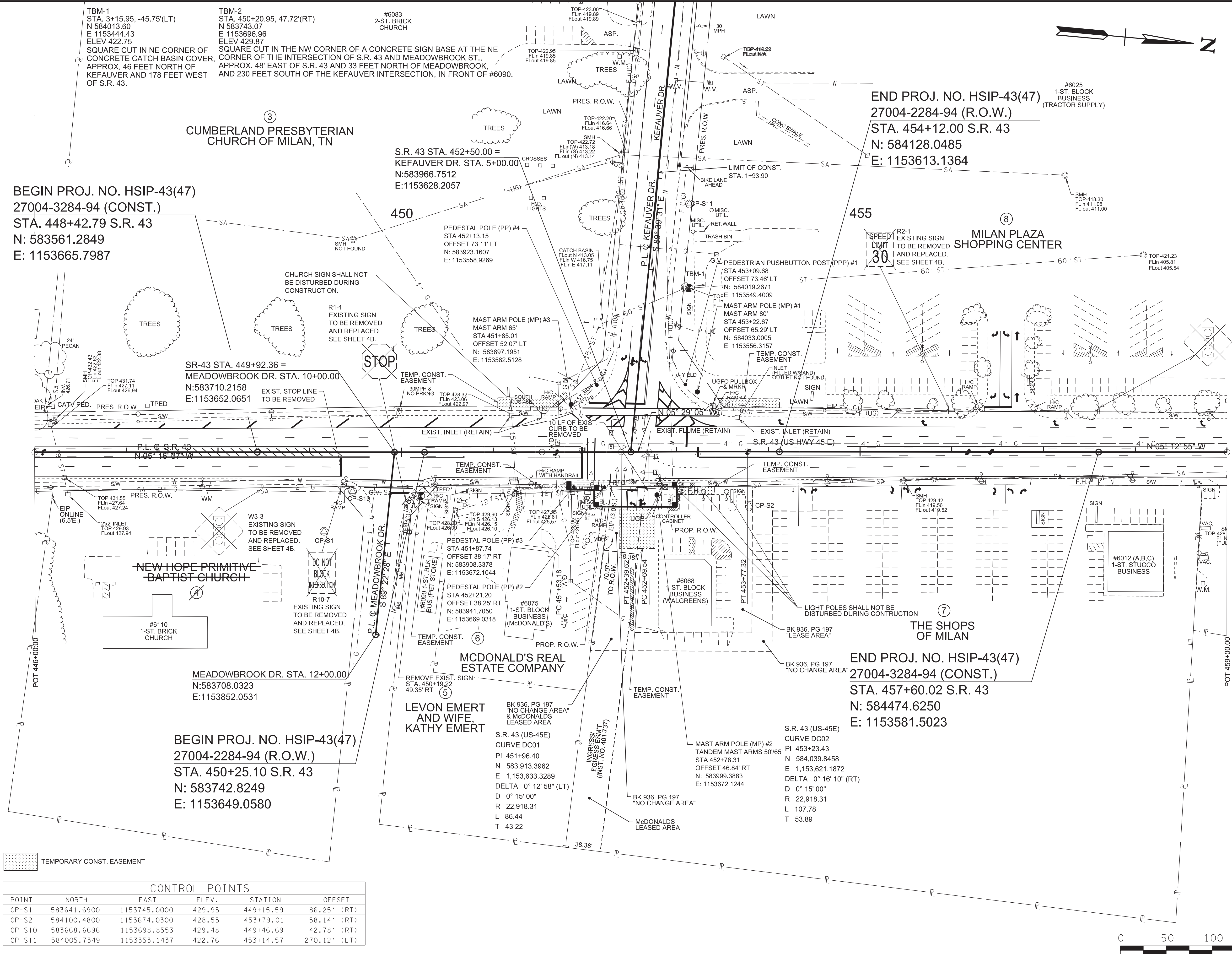
SEALED BY

COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

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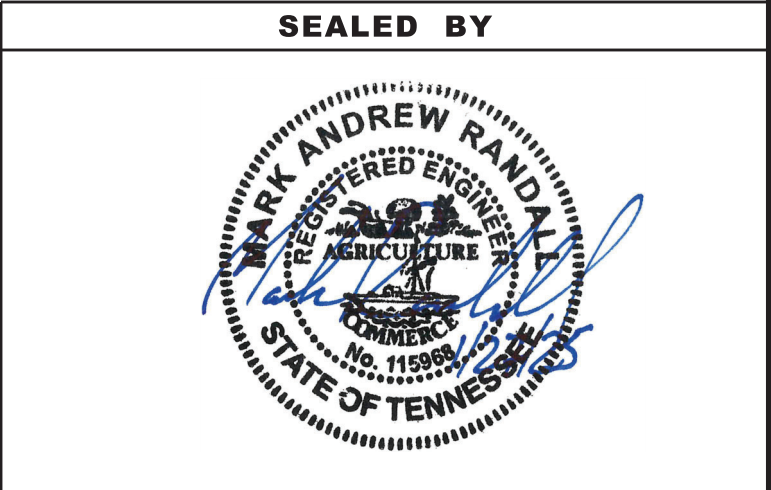
PROPERTY
MAP

STA. 448+42.79 TO STA. 457+60.02
SCALE: 1"=100'



TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	4
PIH	2024	HSIP-43(47)	4
PS&E	2025	HSIP-43(47)	4

REV. 08-08-23: CHANGED TRACT 6 PROPERTY OWNER INFORMATION.
REV. 04-01-24: CHANGED TRACT 6 PROPERTY OWNER INFORMATION.
REV. 09-13-24: ADDED NOTE TO REMOVE EXIST. SIGN STA. 450+19.22. RELOCATED MP#2.



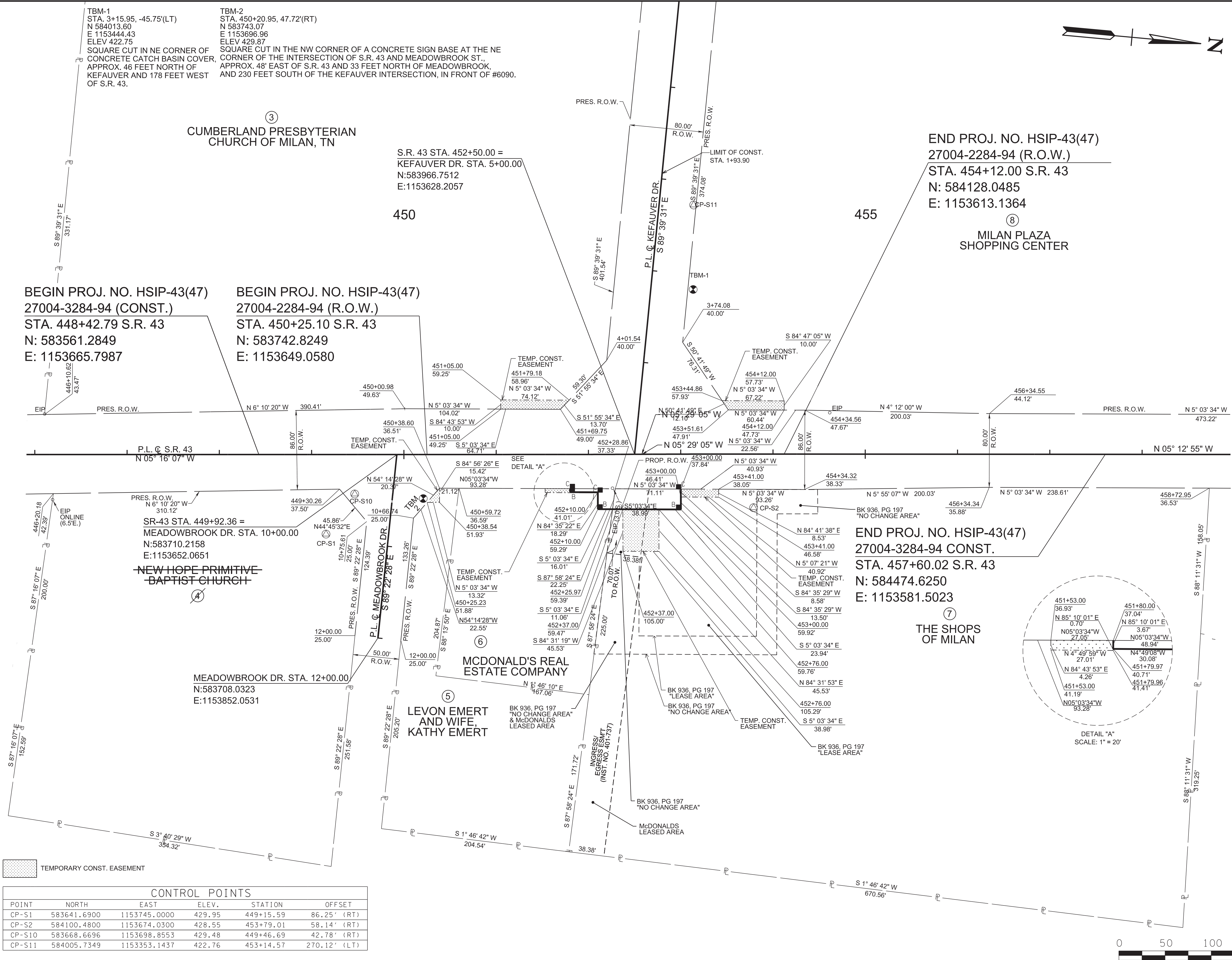
COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

STATE OF TENNESSEE
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PRESENT LAYOUT

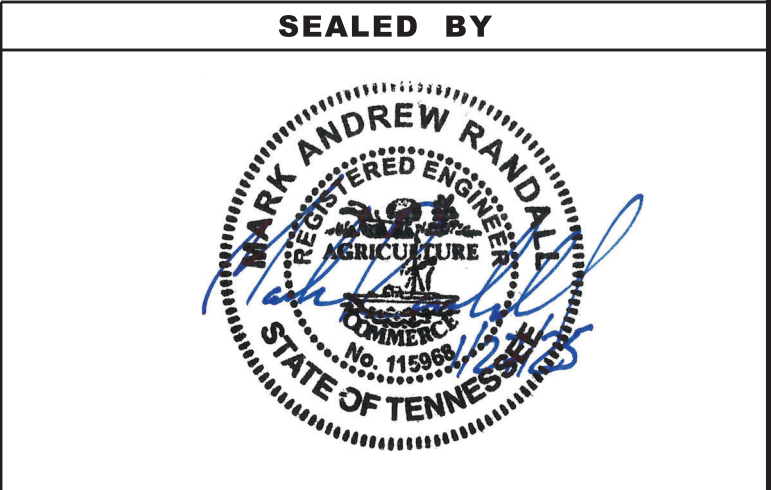
STA.448+42.79 TO STA.457+60.02
SCALE: 1"=50'

1/6/2025
P:\2020\200145\SalInfrastructure\Cad\Dgm\004A.sht



TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	4A
PIH	2024	HSIP-43(47)	4A
PS&E	2025	HSIP-43(47)	4A

REV. 08-08-23: CHANGED TRACT 6 PROPERTY OWNER INFORMATION.
REV. 04-01-24: CHANGED TRACT 6 PROPERTY OWNER INFORMATION.



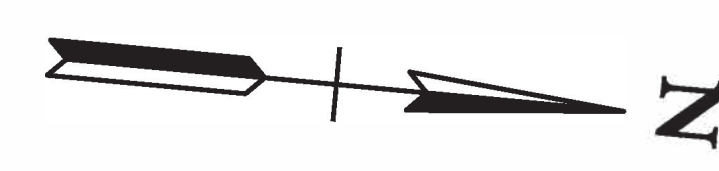
COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

RIGHT OF WAY
DETAILS

STA. 448+42.79 TO STA. 457+60.02
SCALE: 1"=50'

REV. 02-20-24: ADDED EXISTING TRACT 6 DRIVEWAYS
REV. 09-13-24: ADJUSTED SIDEWALK TO ACCOMMODATE UTILITY POLE IN THE NORTHEAST QUADRANT AT KEFAUVER DR. RELOCATED MP#2.



0 50 100 150

STA. 448+42.79 TO STA. 457+60.02
SCALE: 1"=50'

ENVIRONMENTAL NOTES

SUBSECTION 3 – EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.
- (2) AREAS TO BE UNDISTURBED SHALL BE CLEARLY MARKED IN THE FIELD BEFORE CONSTRUCTION ACTIVITIES BEGIN.
- (3) UNLESS OTHERWISE NOTED IN THE PLANS, THE CONTRACTOR SHALL NOT CLEAR/DISTURB ANY AREA BEYOND 15 FEET FROM SLOPE LINES.
- (4) PRE-CONSTRUCTION VEGETATIVE GROUND COVER SHALL NOT BE DESTROYED, REMOVED OR DISTURBED (I.E. CLEARING AND GRUBBING INITIATED) MORE THAN 14 CALENDAR DAYS PRIOR TO GRADING OR EARTH MOVING ACTIVITIES UNLESS THE AREA IS MULCHED, SEEDED WITH MULCH, OR OTHER TEMPORARY COVER IS APPLIED.
- (5) CLEARING, GRUBBING, AND OTHER DISTURBANCE TO RIPARIAN VEGETATION SHALL BE LIMITED TO THE MINIMUM NECESSARY FOR SLOPE CONSTRUCTION AND EQUIPMENT OPERATIONS. EXISTING VEGETATION, INCLUDING STREAM AND WETLAND BUFFERS (UNLESS PERMITTED), SHOULD BE PRESERVED TO THE MAXIMUM EXTENT POSSIBLE. UNNECESSARY VEGETATION REMOVAL IS PROHIBITED.

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (7) TEMPORARY EPSC MEASURES MAY BE REMOVED AT THE BEGINNING OF THE WORKDAY BUT MUST BE REINSTALLED AT THE END OF THE WORKDAY OR BEFORE/DURING A PRECIPITATION EVENT.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.
- (9) OFFSITE VEHICLE TRACKING OF SEDIMENTS AND THE GENERATION OF DUST SHALL BE MINIMIZED. A STABILIZED CONSTRUCTION EXIT (A POINT OF ENTRANCE/EXIT TO THE CONSTRUCTION PROJECT) SHALL BE PROVIDED TO REDUCE THE TRACKING OF MUD AND DIRT ONTO PUBLIC ROADS BY CONSTRUCTION VEHICLES.
- (10) THE DEWATERING OF WORK AREAS, TRENCHES, FOUNDATIONS, EXCAVATIONS, ETC. THAT HAVE COLLECTED STORMWATER, WATER FROM VEHICLE WASH AREAS, OR GROUNDWATER SHALL BE EITHER HELD IN SETTLING BASINS OR TREATED BY FILTRATION AND/OR CHEMICAL TREATMENT PRIOR TO ITS DISCHARGE. ALL PHYSICAL AND/OR CHEMICAL TREATMENT WILL BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S GUIDELINES AND FULLY DESCRIBED IN THE EPSC PLANS. WATER DISCHARGED SHALL NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITHIN THE RECEIVING NATURAL RESOURCE. WATER MUST BE HELD IN SETTLING BASINS UNTIL AT LEAST AS CLEAR AS THE RECEIVING WATERS. SETTLING BASINS SHALL NOT BE LOCATED CLOSER THAN 20 FEET FROM THE TOP BANK OF A STREAM. SETTLING BASINS AND SEDIMENT TRAPS SHALL BE PROPERLY DESIGNED ACCORDING TO THE SIZE OF THE DRAINAGE AREAS OR VOLUME OF WATER TO BE TREATED. TREATED WATER MUST BE DISCHARGED THROUGH A PIPE OR WELL-VEGETATED OR LINED CHANNEL, SO THAT THE DISCHARGE DOES NOT CAUSE EROSION OR SEDIMENT TRANSPORT. DISCHARGES FROM BASINS AND IMPOUNDMENTS SHALL UTILIZE OUTLET STRUCTURES THAT ONLY WITHDRAW WATER FROM NEAR THE SURFACE OF THE BASIN OR IMPOUNDMENT. DISCHARGES MUST NOT CAUSE AN OBJECTIONABLE COLOR CONTRAST WITH THE RECEIVING STREAM.

INSPECTION, MAINTENANCE & REPAIR

- (12) THE TDOT CONSTRUCTION SUPERVISOR (OR THEIR DESIGNEE) AND THE CONTRACTOR'S RESPONSIBLE PARTY ARE RESPONSIBLE FOR INSPECTIONS. MAINTENANCE AND REPAIR ACTIVITIES ARE THE RESPONSIBILITY OF THE CONTRACTOR. THE TDOT CONSTRUCTION SUPERVISOR OR THEIR DESIGNEE SHALL COMPLETE THE EPSC INSPECTION REPORTS AND DISTRIBUTE COPIES PER THE CONTRACT.
- (13) TDOT CONSULTANTS AND CONTRACTOR STAFF RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDEC “LEVEL 1 - FUNDAMENTALS OF EROSION PREVENTION AND SEDIMENT CONTROL FOR CONSTRUCTION SITES” COURSE AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION. TDOT STAFF AND SUPERVISORS RESPONSIBLE FOR THE INSPECTION, IMPLEMENTATION, MAINTENANCE, AND/OR REPAIR OF EPSC MEASURES SHALL SUCCESSFULLY COMPLETE THE TDOT “FUNDAMENTALS OF EROSION AND SEDIMENT CONTROL” CLASS AND ANY REFRESHER COURSES AS REQUIRED TO MAINTAIN CERTIFICATION.
- (14) EPSC CONTROLS SHALL BE INSPECTED ACCORDING TO PERMIT REQUIREMENTS TO VERIFY MEASURES HAVE BEEN INSTALLED AND MAINTAINED IN ACCORDANCE WITH TDOT STANDARD DRAWINGS, SPECIFICATIONS, AND GOOD ENGINEERING PRACTICES. EPSC INSPECTIONS SHALL BE DOCUMENTED ON THE TDOT EPSC INSPECTION REPORT.
- (15) DISCHARGE POINTS SHALL BE INSPECTED TO ASCERTAIN WHETHER EPSC MEASURES ARE EFFECTIVE IN PREVENTING EROSION AND CONTROLLING SEDIMENT INCLUDING SIGNIFICANT IMPACTS TO SURROUNDING NATURAL RESOURCES AND ADJACENT PROPERTY OWNERS. WHERE DISCHARGE LOCATIONS ARE INACCESSIBLE, NEARBY DOWN GRADIENT LOCATIONS SHALL BE INSPECTED. LOCATIONS WHERE VEHICLES ENTER AND EXIT THE SITE SHALL BE INSPECTED FOR EVIDENCE OF OFFSITE ROADWAY SEDIMENT TRACKING.
- (16) UPON CONCLUSION OF THE INSPECTIONS, EPSC MEASURES FOUND TO BE INEFFECTIVE SHALL BE REPAIRED, REPLACED, OR MODIFIED BEFORE THE NEXT RAIN EVENT, IF POSSIBLE, BUT IN NO CASE MORE THAN 24 HOURS AFTER THE INSPECTION OR WHEN THE CONDITION IS IDENTIFIED. IF THE REPAIR, REPLACEMENT OR MODIFICATION IS NOT PRACTICAL WITHIN THE 24 HOUR TIMEFRAME, WRITTEN DOCUMENTATION SHALL BE PROVIDED IN THE FIELD DIARY AND EPSC INSPECTION REPORT. AN ESTIMATED REPAIR, REPLACEMENT OR MODIFICATION SCHEDULE SHALL BE DOCUMENTED WITHIN 24 HOURS AFTER IDENTIFICATION.
- (17) INSPECTION, REPAIR, AND MAINTENANCE OF EPSC MEASURES SHALL BE PERFORMED ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM SEDIMENT CONTROL STRUCTURES WHEN THE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT (50%). DURING SEDIMENT REMOVAL, THE CONTRACTOR SHALL TAKE STEPS TO ENSURE THAT STRUCTURAL COMPONENTS OF EPSC MEASURES ARE NOT DAMAGED AND THUS MADE INEFFECTIVE. IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL REPAIR THE EPSC MEASURES AT THE CONTRACTOR'S OWN EXPENSE.
- (18) THE EPSC PLAN SHALL BE UPDATED WHENEVER EPSC INSPECTIONS INDICATE, OR WHERE STATE OR FEDERAL OFFICIALS DETERMINE EPSC MEASURES ARE PROVING INEFFECTIVE IN ELIMINATING OR SIGNIFICANTLY MINIMIZING POLLUTANT SOURCES OR ARE OTHERWISE NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS IN STORM WATER DISCHARGES ASSOCIATED WITH THE CONSTRUCTION ACTIVITY.
- (19) SEDIMENT REMOVED FROM SEDIMENT CONTROL STRUCTURES SHALL BE PLACED AND TREATED IN A MANNER SO THAT THE SEDIMENT IS CONTAINED WITHIN THE PROJECT LIMITS AND DOES NOT MIGRATE ONTO ADJACENT PROPERTIES AND INTO WATERS OF THE STATE/U.S. COST FOR THIS TREATMENT SHALL BE INCLUDED IN PRICE BID FOR ITEM NO. 209-05 SEDIMENT REMOVAL, C.Y.

EROSION PREVENTION

- (20) CONSTRUCTION SHALL BE SEQUENCED AND STAGED TO MINIMIZE THE EXPOSURE TIME OF GRADED OR DENUDED SOIL AREAS, PRESERVE TOPSOIL, AND MINIMIZE SOIL COMPACTION.
- (21) THE ACCEPTED EPSC PLAN SHALL REQUIRE THAT EPSC MEASURES BE IN PLACE BEFORE CLEARING, GRUBBING, EXCAVATION, GRADING, CULVERT OR BRIDGE CONSTRUCTION, CUTTING, FILLING, OR ANY OTHER EARTHWORK OCCURS, EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.

- (22) NO WORK SHALL BE STARTED UNTIL THE CONTRACTOR'S PLAN FOR THE STAGING OF OPERATIONS, INCLUDING THE PLAN FOR STAGING OF TEMPORARY AND PERMANENT EPSC MEASURES, HAS BEEN ACCEPTED BY THE TDOT RESPONSIBLE PARTY. THE CONTRACTOR'S EPSC PLAN SHALL INCORPORATE AND SUPPLEMENT, AS ACCEPTABLE, THE BASIC EPSC DEVICES ON THE EPSC PLAN.
- (23) TEMPORARY STABILIZATION SHALL BE INITIATED WITHIN 14 DAYS (2 WEEKS) WHEN CONSTRUCTION ACTIVITIES ON A PORTION OF THE SITE ARE TEMPORARILY CEASED AND EARTH DISTURBING ACTIVITIES WILL NOT RESUME UNTIL AFTER 14 DAYS (2 WEEKS). PERMANENT STABILIZATION MEASURES IN DISTURBED AREAS SHALL BE INITIATED WITHIN 14 DAYS (2 WEEKS) AFTER FINAL GRADING OF ANY PHASE OF CONSTRUCTION.
- (24) STEEP SLOPES SHALL BE TEMPORARILY STABILIZED NOT LATER THAN 7 DAYS (A WEEK) AFTER CONSTRUCTION ACTIVITY ON THE SLOPE HAS TEMPORARILY OR PERMANENTLY CEASED. STEEP SLOPES ARE DEFINED AS A NATURAL OR CREATED SLOPE OF 35% GRADE OR GREATER REGARDLESS OF HEIGHT.
- (25) PERMANENT STABILIZATION WILL REPLACE TEMPORARY MEASURES AS SOON AS PRACTICABLE. PRIORITY SHALL BE GIVEN TO FINISHING OPERATIONS AND PERMANENT EPSC MEASURES OVER TEMPORARY EPSC MEASURES ON ALL PROJECTS.
- (26) TEMPORARY OR PERMANENT STABILIZATION MUST BE FREE OF FINES (SILT AND CLAY SIZED PARTICLES). UNPACKED GRAVEL CONTAINING FINES OR CRUSHER-RUN WILL NOT BE CONSIDERED SUFFICIENT STABILIZATION.
- (27) DELAYING THE PLANTING OF COVER VEGETATION UNTIL WINTER MONTHS OR DRY MONTHS SHOULD BE AVOIDED.

PERMITS, PLANS & RECORDS

- (28) THE EPSC PLAN IS TO SERVE AS AN INITIAL GUIDE FOR SITE PERSONNEL AS THE CONSTRUCTION PROCESS DEVELOPS. IT MUST BE AMENDED, MODIFIED, AND UPDATED WHENEVER A CHANGE IN THE DESIGN OR CONSTRUCTION OF THE PROJECT OCCURS. THE STAGES DEPICTED IN THE EPSC PLANS MAY NOT COINCIDE WITH THE ACTUAL PHASES OF CONSTRUCTION ESTABLISHED BY THE CONTRACTOR DURING CONSTRUCTION, THUS MODIFICATIONS WILL BE REQUIRED TO ENSURE THE EPSC PLAN IS MAINTAINED TO DEPICT CURRENT SITE CONDITIONS. IT SHOULD BE MAINTAINED SUCH THAT IT WILL ALWAYS REFLECT THE MEASURES THAT ARE INSTALLED DURING THE VARIOUS PHASES OF CONSTRUCTION. IT IS IMPRACTICAL TO DETERMINE ALL THE INTERMEDIATE PHASES OF CONSTRUCTION THAT WILL OCCUR; THUS THESE DOCUMENTS WILL HAVE TO BE UPDATED THROUGHOUT THE LIFE OF THE CONSTRUCTION PROJECT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2024	HSIP-43(47)	5
PIH	2024	HSIP-43(47)	5
PS&E	2025	HSIP-43(47)	5

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DEPARTMENT OF TRANSPORTATION

EROSION
PREVENTION
AND SEDIMENT
CONTROL NOTES

ENVIRONMENTAL NOTES

- (33)

IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34)

ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35)

WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36)

ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37)

ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38)

OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39)

DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40)

WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SUPPORT ACTIVITIES

- (41)

IF OFFSITE BORROW AND WASTE AREAS BECOME NECESSARY DURING THE LIFE OF THE PROJECT, THIS SUPPORT ACTIVITY SHALL BE ADDRESSED PER THE TDOT WASTE AND BORROW MANUAL.
- (42)

MATERIALS AND STAGING AREAS SHALL BE LOCATED IN NON-WETLAND AREAS AND ABOVE THE 100-YEAR, FEDERAL EMERGENCY MANAGEMENT AGENCY FLOODPLAIN.
- (43)

IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO SUPPLY EPSC PLANS FOR THE MATERIAL AND STAGING AREAS TO THE ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE FOR REVIEW.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44)

ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45)

FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46)

APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.

- (47)

ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48)

THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49)

IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50)

FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51)

IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52)

WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53)

CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

SUBSECTION 4 – EROSION PREVENTION AND SEDIMENT CONTROL SPECIAL NOTES

STREAMS, WETLANDS & BUFFER ZONES

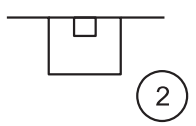
- (1)

FOR PROJECTS THAT DISCHARGE INTO KNOWN EXCEPTIONAL TENNESSEE WATERS OR WATERS IMPAIRED BY SILTATION, A 60 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM WITH THIS DESIGNATION SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 60 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 30 FEET AT ANY MEASURED LOCATION.
- (2)

A 30 FOOT NATURAL RIPARIAN BUFFER ZONE ADJACENT TO AND ON BOTH SIDES OF THE RECEIVING STREAM SHALL BE PRESERVED TO THE MAXIMUM EXTENT PRACTICABLE DURING CONSTRUCTION ACTIVITIES AT THE SITE. THE 30 FOOT CRITERION FOR THE WIDTH OF THE BUFFER ZONE CAN BE ESTABLISHED ON AN AVERAGE WIDTH BASIS AT A PROJECT, AS LONG AS THE MINIMUM WIDTH OF THE BUFFER ZONE IS MORE THAN 15 FEET AT ANY MEASURED LOCATION. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES.
- (3)

BUFFER ZONES ARE NOT SEDIMENT CONTROL MEASURES AND MUST NOT BE RELIED UPON AS PRIMARY SEDIMENT CONTROL MEASURES. THE RIPARIAN BUFFER ZONE SHALL BE ESTABLISHED BETWEEN THE TOP OF THE STREAM BANK AND THE DISTURBED CONSTRUCTION AREA. EVERY ATTEMPT SHALL BE MADE FOR CONSTRUCTION ACTIVITIES NOT TO TAKE PLACE WITHIN THE BUFFER ZONES. BEST MANAGEMENT PRACTICES (BMPs) PROVIDING EQUIVALENT PROTECTION AS THE NATURAL RIPARIAN ZONE MAY BE USED. WHERE ISSUED, ARAP/401 REQUIREMENTS WILL PREVAIL IF IN CONFLICT WITH THESE BUFFER ZONE REQUIREMENTS.

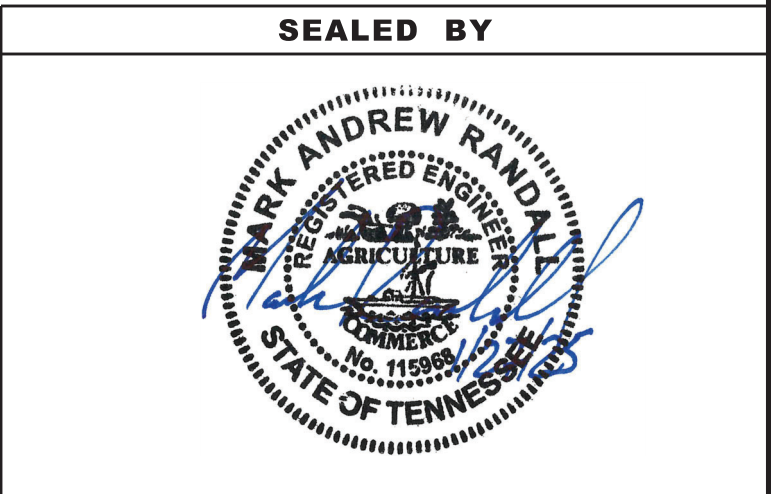
TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	6
PIH	2024	HSIP-43(47)	6
PS&E	2025	HSIP-43(47)	6

EROSION PREVENTION AND SEDIMENT CONTROL LEGEND		
SYMBOL	ITEM	STD. DWG.
**TUBE 12" **TUBE 12" **	12 INCH SEDIMENT TUBE	EC-STR-37
	CURB INLET PROTECTION (TYPE 2)	EC-STR-39

EROSION PREVENTION AND SEDIMENT CONTROL QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
(1) 209-05	SEDIMENT REMOVAL	CY	5
(1) (2) 209-09.41	CURB INLET PROTECTION (TYPE 2)	EA	6
(1) (2) 740-11.02	TEMPORARY SEDIMENT TUBE 12IN	LF	690

FOOTNOTES:
1) SEE SUBSECTION 209.07 OF THE STANDARD SPECIFICATIONS FOR MAINTENANCE REPLACEMENT.

2) TO BE USED AS DIRECTED BY ENGINEER.

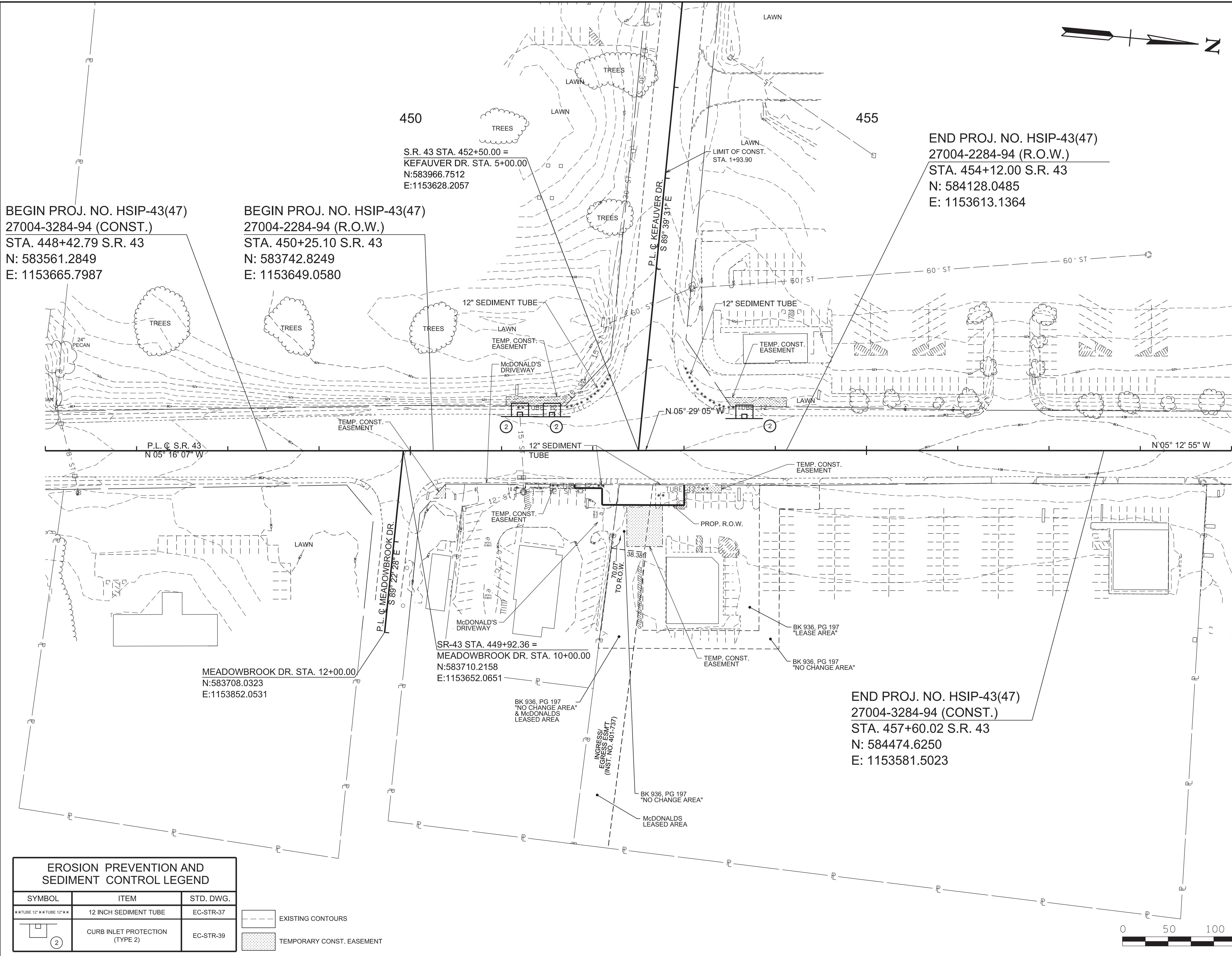


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EPSC
SPECIAL NOTES,
LEGEND, &
TABULATION

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	7
PIH	2024	HSIP-43(47)	7
PS&E	2025	HSIP-43(47)	7

REV. 02-20-24: ADDED MCDONALD'S DRIVEWAY LABELS



EROSION PREVENTION AND SEDIMENT CONTROL LEGEND		
SYMBOL	ITEM	STD. DWG.
	12 INCH SEDIMENT TUBE	EC-STR-37
	CURB INLET PROTECTION (TYPE 2)	EC-STR-39

	EXISTING CONTOURS
	TEMPORARY CONST. EASEMENT

STAGE I

SEALED BY

COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EROSION PREVENTION & SEDIMENT CONTROL (EPSC) PLANS
STA.448+42.79 TO STA.457+60.02
SCALE: 1"=50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	8
PIH	2024	HSIP-43(47)	8
PS&E	2025	HSIP-43(47)	8

REV. 02-20-24: ADDED EXISTING TRACT 6 DRIVEWAYS

BEGIN PROJ. NO. HSIP-43(47)
27004-3284-94 (CONST.)
STA. 448+42.79 S.R. 43
N: 583561.2849
E: 1153665.7987

BEGIN PROJ. NO. HSIP-43(47)
27004-2284-94 (R.O.W.)
STA. 450+25.10 S.R. 43
N: 583742.8249
E: 1153649.0580

S.R. 43 STA. 452+50.00 =
KEFAUVER DR. STA. 5+00.00
N:583966.7512
E:1153628.2057

450

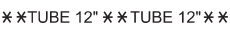
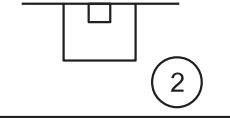
455


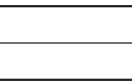
END PROJ. NO. HSIP-43(47)
27004-2284-94 (R.O.W.)
STA. 454+12.00 S.R. 43
N: 584128.0485
E: 1153613.1364

SR-43 STA. 449+92.36 =
MEADOWBROOK DR. STA. 10+00.00
N:583710.2158
E:1153652.0651

MEADOWBROOK DR. STA. 12+00.00
N:583708.0323
E:1153852.0531

END PROJ. NO. HSIP-43(47)
27004-3284-94 (CONST.)
STA. 457+60.02 S.R. 43
N: 584474.6250
E: 1153581.5023

EROSION PREVENTION AND SEDIMENT CONTROL LEGEND		
SYMBOL	ITEM	STD. DWG.
	12 INCH SEDIMENT TUBE	EC-STR-37
	CURB INLET PROTECTION (TYPE 2)	EC-STR-39

-  TEMPORARY CONST. EASEMENT
-  PROP. CONTOURS



STAGE II

SEALED BY



COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EROSION
PREVENTION &
SEDIMENT CONTROL
(EPSC) PLANS
STA.448+42.79 TO STA.457+60.02
SCALE: 1"=50'

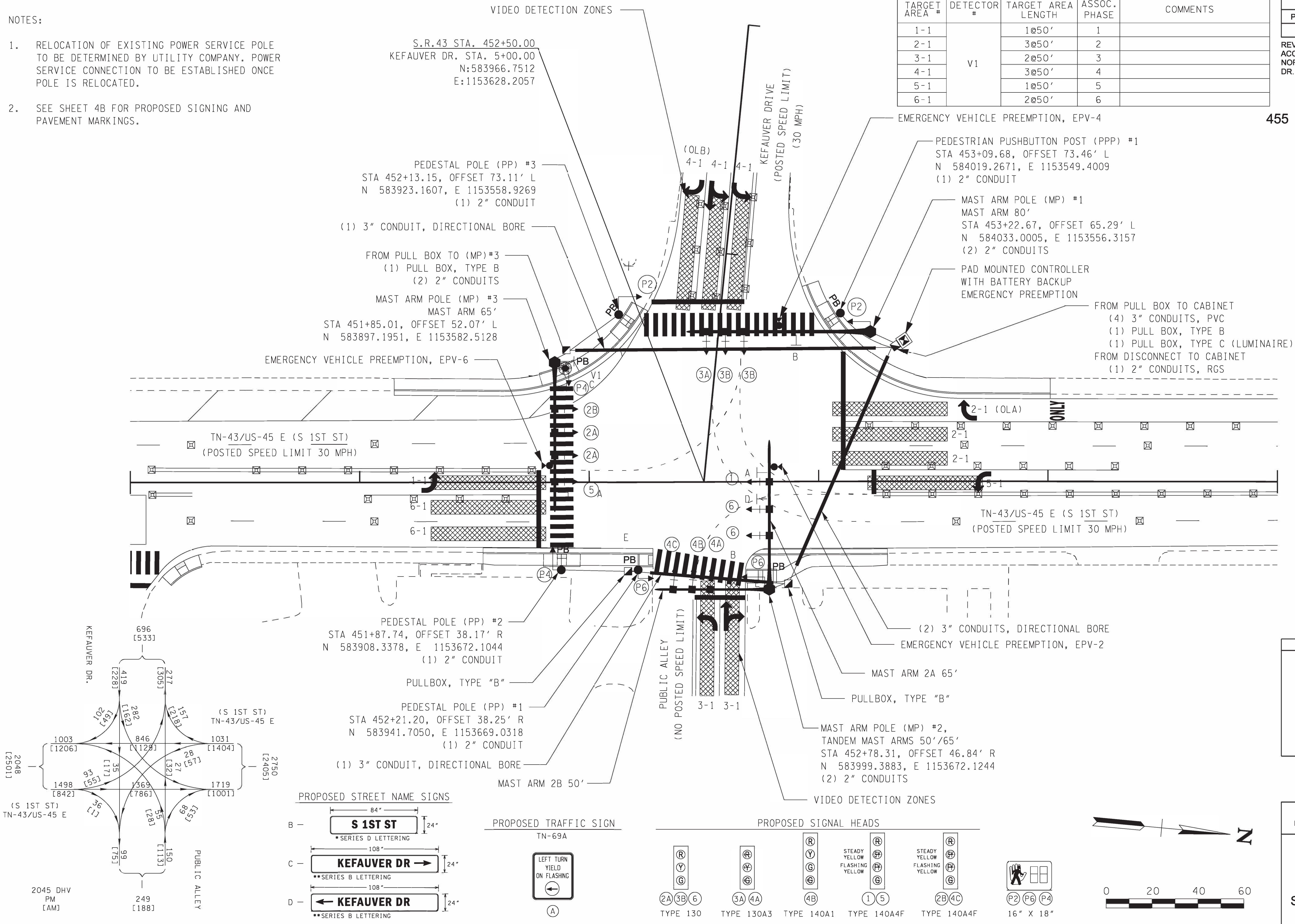
NOTES:

1. RELOCATION OF EXISTING POWER SERVICE POLE TO BE DETERMINED BY UTILITY COMPANY. POWER SERVICE CONNECTION TO BE ESTABLISHED ONCE POLE IS RELOCATED.
2. SEE SHEET 4B FOR PROPOSED SIGNING AND PAVEMENT MARKINGS.

VIDEO DETECTION TARGET ASSIGNMENT				
TARGET AREA #	DETECTOR #	TARGET AREA LENGTH	ASSOC. PHASE	COMMENTS
1-1	V1	1@50'	1	
2-1		3@50'	2	
3-1		2@50'	3	
4-1		3@50'	4	
5-1		1@50'	5	
6-1		2@50'	6	

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	9
PIH	2024	HSIP-43(47)	9
PS&E	2025	HSIP-43(47)	9

REV. 09-13-24: ADJUSTED SIDEWALK TO ACCOMMODATE UTILITY POLE IN THE NORTHEAST QUADRANT AT KEFAUVER DR. RELOCATED MP#2.



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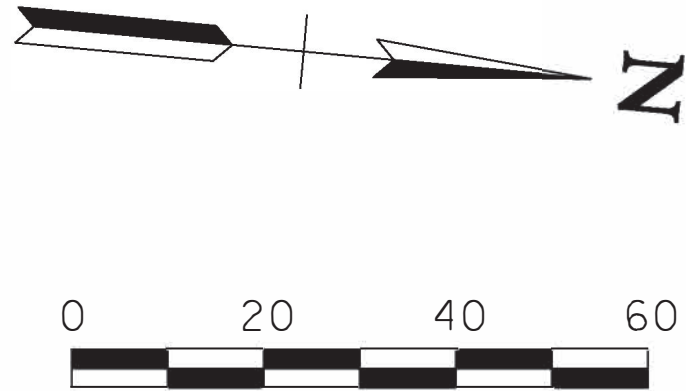
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COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID 12B.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

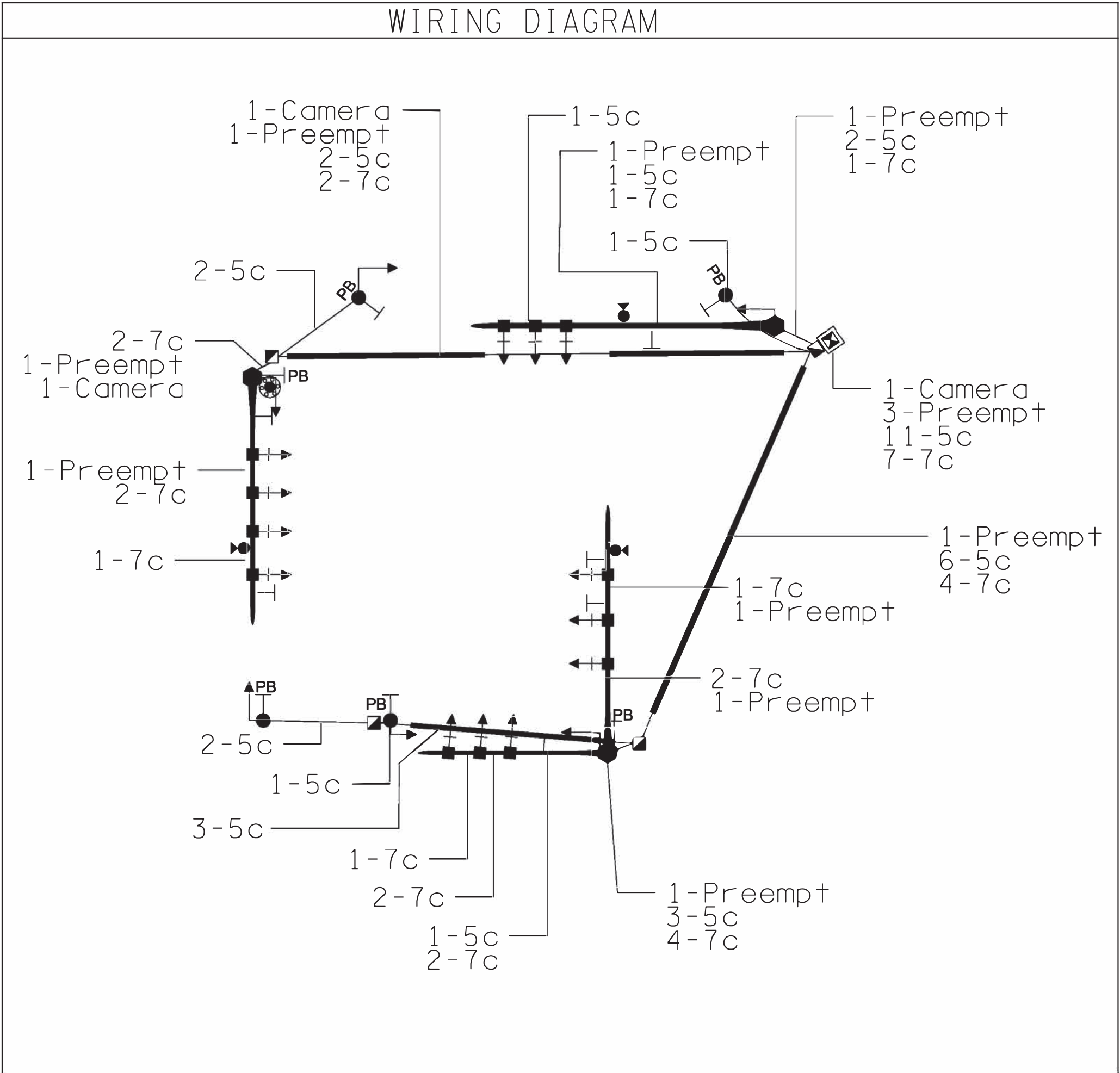
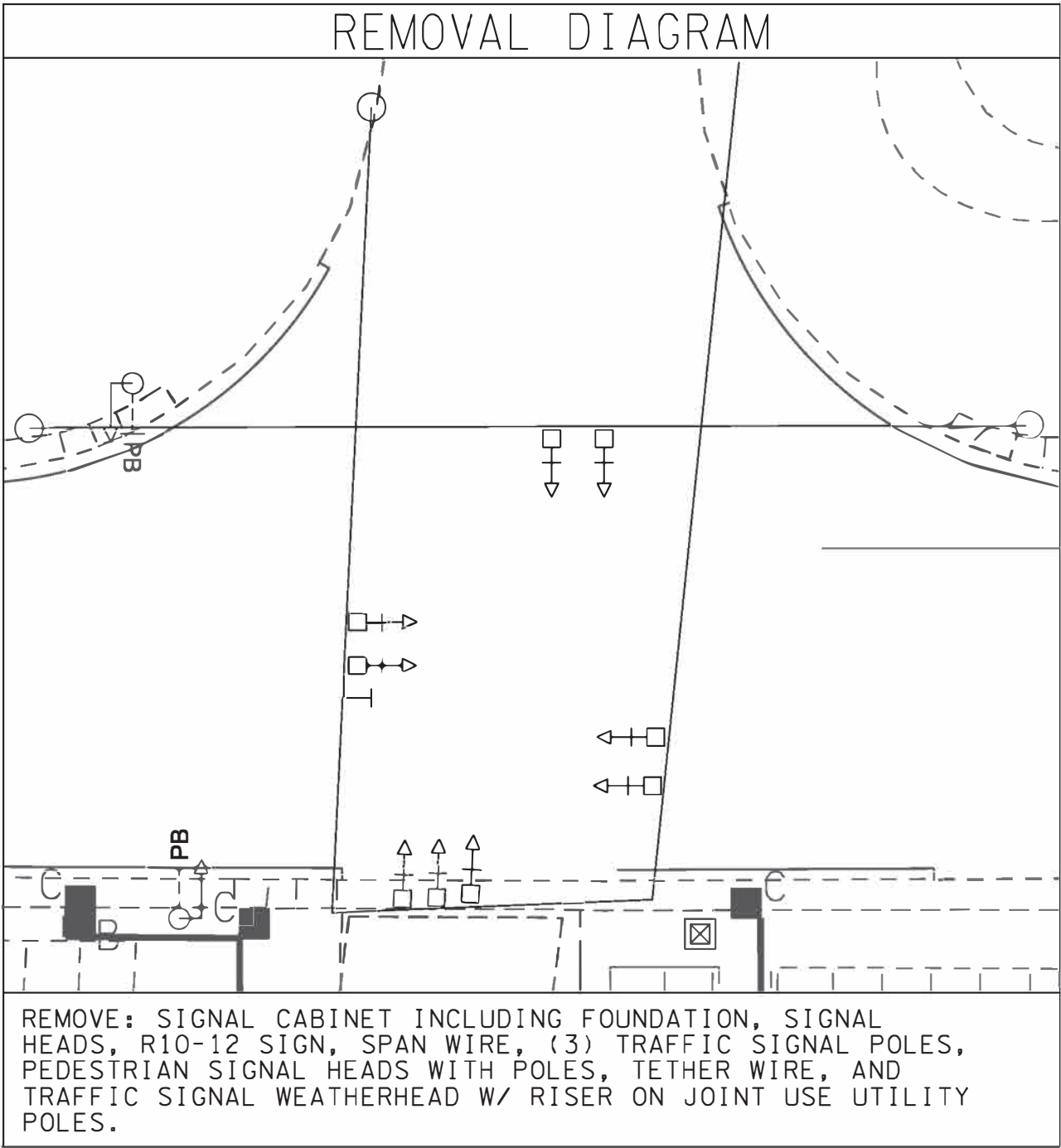
TRAFFIC SIGNAL
LAYOUT

STA.450+00 TO STA.455+00
SCALE: 1"=20'



TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	10
PIH	2024	HSIP-43(47)	10
PS&E	2025	HSIP-43(47)	10

REV. 09-13-24: ADJUSTED SIDEWALK TO ACCOMMODATE UTILITY POLE IN THE NORTHEAST QUADRANT AT KEFAUVER DR. RELOCATED MP#2.



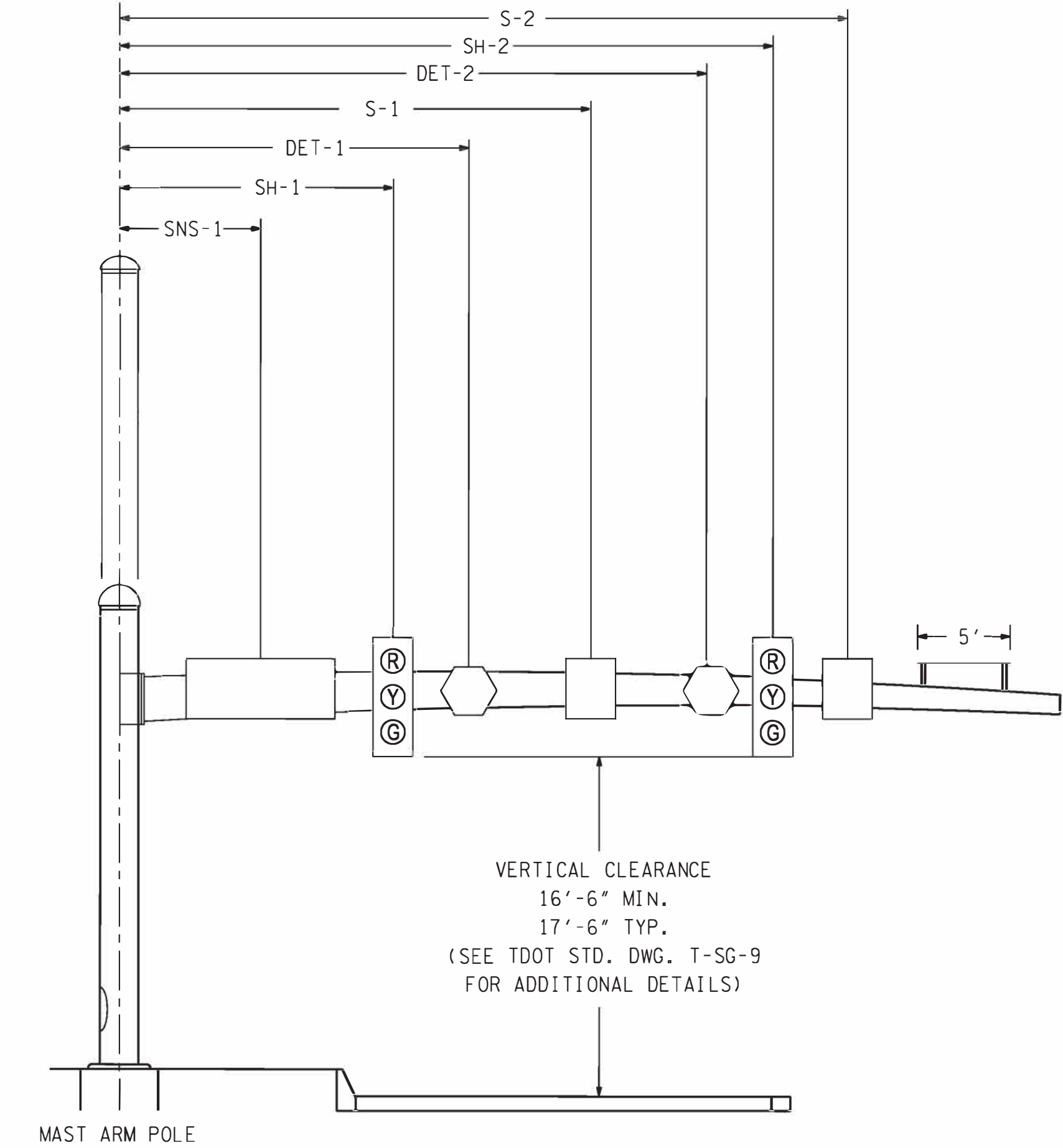
VIDEO DETECTION TARGET ASSIGNMENT				
TARGET AREA #	DETECTOR #	TARGET AREA LENGTH	ASSOC. PHASE	COMMENTS
1-1	V1	1@50'	1	
2-1		3@50'	2	
3-1		2@50'	3	
4-1		3@50'	4	
5-1		1@50'	5	
6-1		2@50'	6	

(1) NL = NON LOCK, L = LOCK (2) PERM = PERMITTED, PROT = PROTECTED, P/P = PROT/PERM

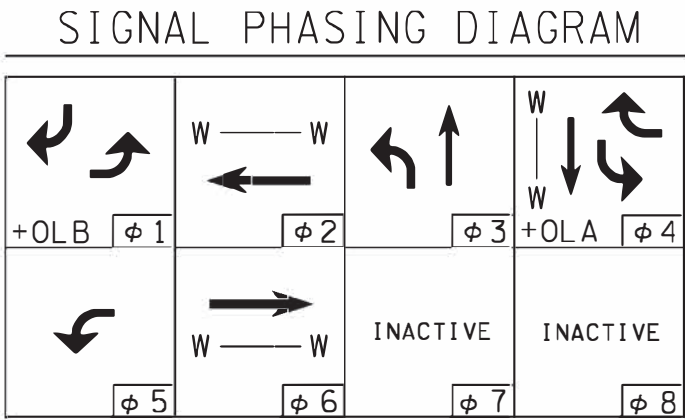
BASIC OR SEMI-ACTUATED TIMINGS (SECONDS)										
PHASE	MINIMUM GREEN	PASSAGE TIME	MAX 1	CLEARANCE		RECALL TO	MEMORY POSITION (1)	LEFT TURN OPERATION (2)	PEDESTRIAN	
				YELLOW	ALL RED				WALK	DON'T WALK
1	5	1.1	25	4.0	2.0	-	NL	P/P	-	-
2	15	1.4	45	4.0	2.0	MIN	NL	-	7	25
3	10	1.1	35	4.0	2.0	-	NL	SPLIT	-	-
4	10	1.1	25	4.0	2.5	-	NL	SPLIT	7	23
5	5	1.1	45	4.0	2.0	-	NL	P/P	-	-
6	15	1.4	35	4.0	2.0	MIN	NL	-	7	14

* THESE TIMINGS ARE INITIAL AND MAY BE ADJUSTED BY THE CONTRACTOR BASED ON FIELD OBSERVATIONS TO PROVIDE EFFICIENT OPERATION.

** WHEN THE CONTROLLER IS IN THE FLASHING MODE, THE VEHICULAR SIGNAL HEADS SHALL FLASH YELLOW ON PHASES 2 & 6, RED ON ALL OTHER PHASES.



LEGEND	
	SIGNAL HEAD (SH-X)
	STREET NAME SIGN (SNS)
	SIGN (S)
	VEHICLE DETECTOR (VD) (VIDEO)
	VIBRATION DAMPENER PLATE

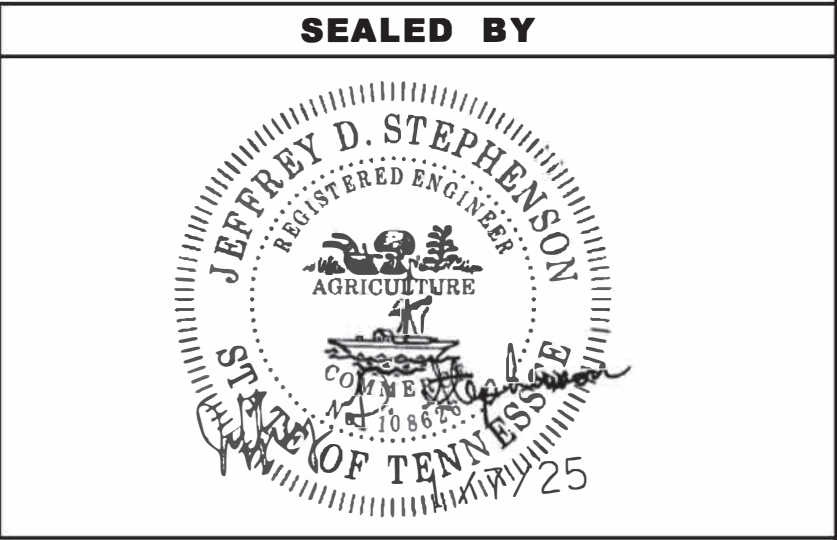


PEDESTRIAN SIGNAL SUPPORT POLE DATA							
POLE NO.	STATION	OFFSET	NORTHING	EASTING	POLE HEIGHT	GROUND EL @ POLE	
PPP-1	453+09.68	L 73.46'	584019.2671	1153549.4009	5'	427.44	
PP-1	452+21.20	R 38.25'	583941.7050	1153669.0318	10'	428.49	
PP-2	451+87.74	R 38.17'	583908.3378	1153672.1044	10'	428.97	
PP-3	452+13.15	L 73.11'	583923.1607	1153558.9269	10'	426.86	

MAST ARM SIGNAL SUPPORT POLE DATA															
POLE NO.	STATION	OFFSET	NORTHING	EASTING	MAST ARM(S)	ARM LENGTH	SIGNAL HEADS				DETECTION DET-1	SNS	SIGNS	PRE	GROUND EL @ POLE
							SH-1	SH-2	SH-3	SH-4					
MP-1	453+22.67	L 65.29'	584033.0005	1153556.3157		80'	55'	63'	71'	-	-	32'	-	40'	427.49
MP-2	452+78.31	R 46.84'	583999.3883	1153672.1244	MP-2A	65'	25'	37'	49'	-	-	41'	54'	56'	428.16
					MP-2B	50'	26'	34'	42'	-	-	17'	-	-	
MP-3	451+85.01	L 52.07'	583897.1951	1153582.5128		65'	20'	30'	40'	52'	ON POLE	10'	53'	46'	427.95

LUMINAIRE ARMS (IF APPLICABLE) SHALL BE 12' IN LENGTH, AND AN ATTACHMENT HEIGHT OF 30' MOUNTED ON STRAIN POLE FOOTING DEPTHS WILL BE BASED ON POLE MAX BENDING MOMENT FROM DESIGN REQUIREMENTS.

THE MAST ARM POLE FOUNDATIONS MAY BE ADJUSTED UP TO 2' TO AVOID UTILITIES. ADJUSTMENTS GREATER THAN 2' MUST BE REVIEWED AND APPROVED BY ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ARM LENGTH IS ADEQUATE FOR PROPER SIGNAL HEAD PLACEMENT PER T-SG-7A THROUGH T-SG-7S.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC SIGNAL
DETAILS

SCALE: N.T.S.

1/6/2025
P:\2020\200145\Salnfrastucture\Cad\Dgm\T1.sht

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
- a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
- a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3.5 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

- a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
- (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.
- d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.
- a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:
- a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:


SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	T1
PIH	2024	HSIP-43(47)	T1
PS&E	2025	HSIP-43(47)	T1

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	T2
PIH	2024	HSIP-43(47)	T2
PS&E	2025	HSIP-43(47)	T2

SEQUENCE OF CONSTRUCTION

PHASE 1

- 1. INSTALL ALL TRAFFIC CONTROL SIGNS AND CHANNELIZING DRUMS.
- 2. CONSTRUCT THE PROPOSED NORTHBOUND FACILITIES ON THE EAST SIDE OF SR-43.

PHASE II

- 1. INSTALL ALL TRAFFIC CONTROL SIGNS AND CHANNELIZING DRUMS.
- 2. CONSTRUCT THE PROPOSED SOUTHBOUND FACILITIES ON THE WEST SIDE OF SR-43.

PHASE III

- 1. INSTALL ALL TRAFFIC CONTROL SIGNS AND CHANNELIZING DRUMS.
- 2. INSTALL SIGNAL AND EQUIPMENT ON S3-43 & KEFAUVER DR.
- 3. INSTALL THE FINAL PAVEMENT MARKING & PAVEMENT MARKERS.
- 4. REMOVE ALL TRAFFIC CONSTROL SIGNS AND DEVICES.


NOTES:

- 1. LANE CLOSURES ARE TO BE TEMPORARY AND LIMITED TO DAY TIME OPERATIONS.
- 2. ACCESS TO RESIDENCES AND BUSINESS MUST BE PROVIDED AT ALL TIMES.

TRAFFIC CONTROL QUANTITIES								
ITEM NO.	DESCRIPTION	M.U.T.C.D. NO.	SIGN DESCRIPTION	UNITS	PHASE 1	PHASE 2	PHASE 3	MAX. OR TOTAL
712-01	TRAFFIC CONTROL			L.S.	1	1	1	1
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)			EA.	31	50	50	50
712-06	SIGNS (CONSTRUCTION)							
		G20-2	END ROAD WORK	S.F.	24	24	24	24
		W20-1	ROAD WORK	S.F.	112	112	144	144
		W4-2R	LANE ENDS (RIGHT)	S.F.	16	16	-	16
		W20-5R	RIGHT LANE CLOSED (WITH DISTANCE)	S.F.	16	16	-	16
712-06	TOTAL SQUARE FOOTAGE SIGNS							200
712-08.03	ARROW BOARD (TYPE C)			EA.	1	1	1	1
713-16.01	CHANGEABLE MESSAGE SIGN UNIT			EA.	3	3	3	3

TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
▨▨▨▨▨	WORK ZONE
●	FLEXIBLE DRUMS (CHANNELIZING)
⬮	SIGN (CONSTRUCTION)
→	TRAFFIC FLOW

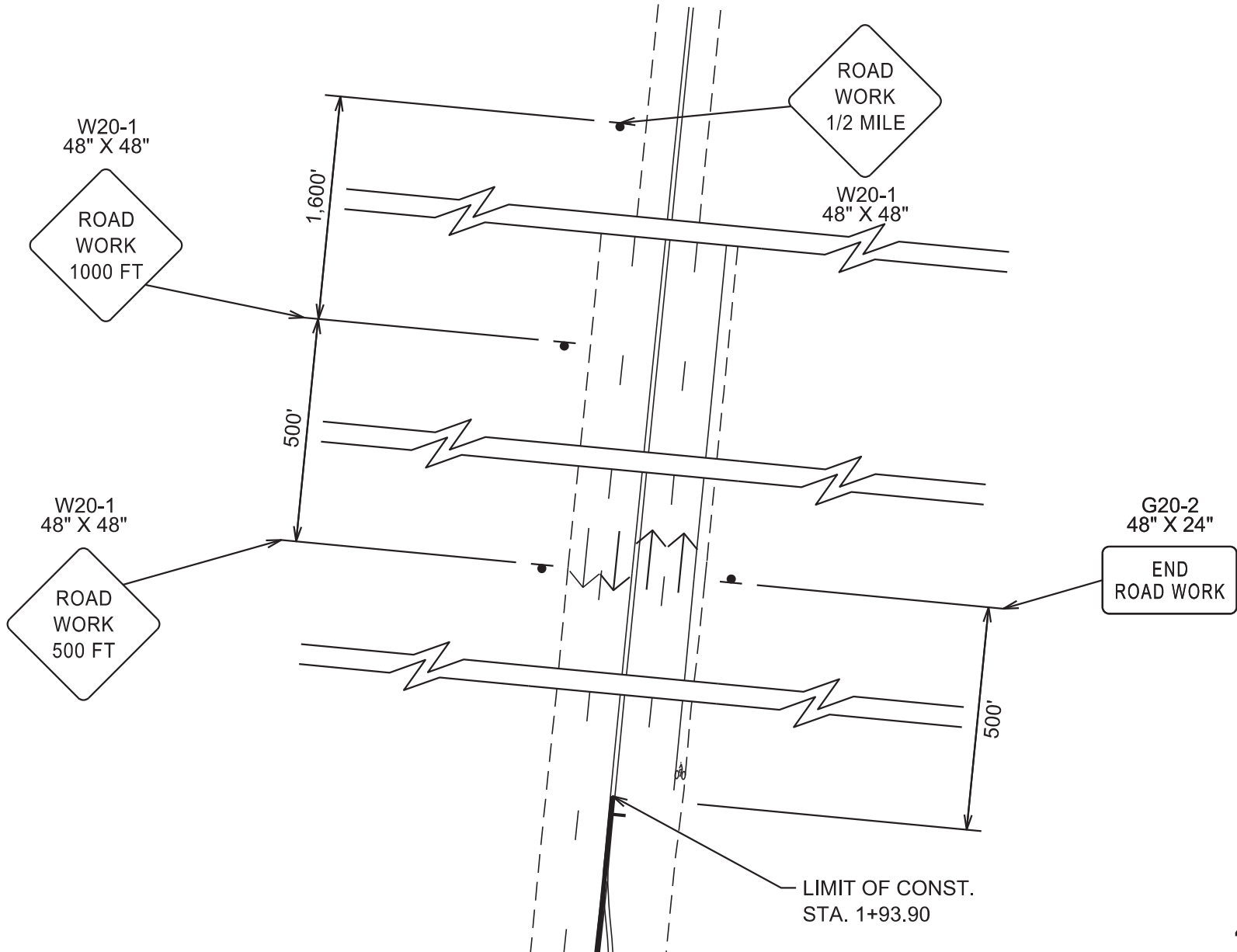
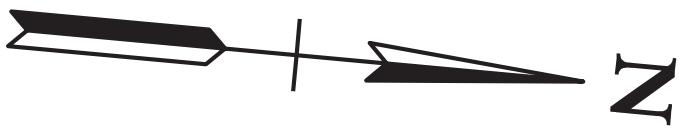
SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL
PHASING NOTES,
LEGEND AND
TABULATION

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	T3
PIH	2024	HSIP-43(47)	T3
PS&E	2025	HSIP-43(47)	T3



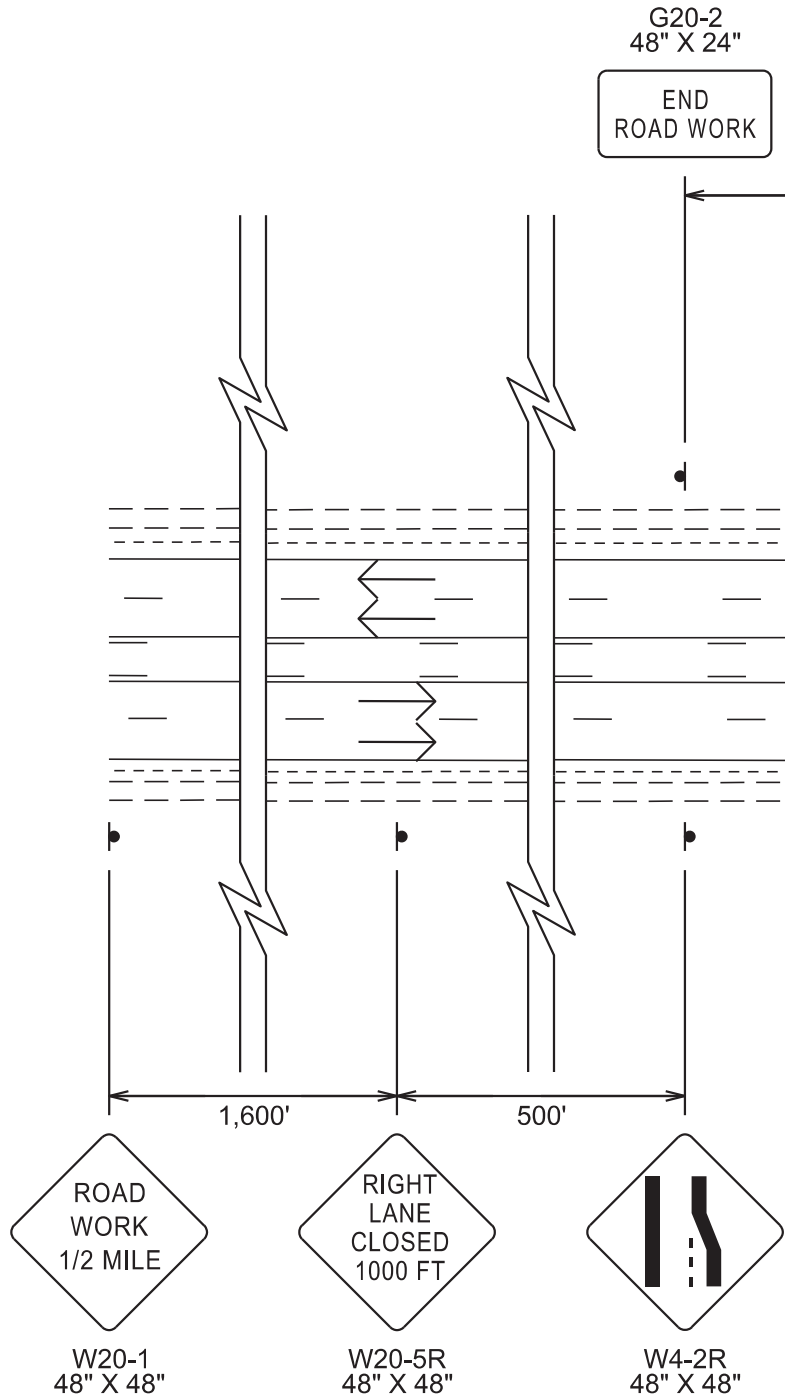
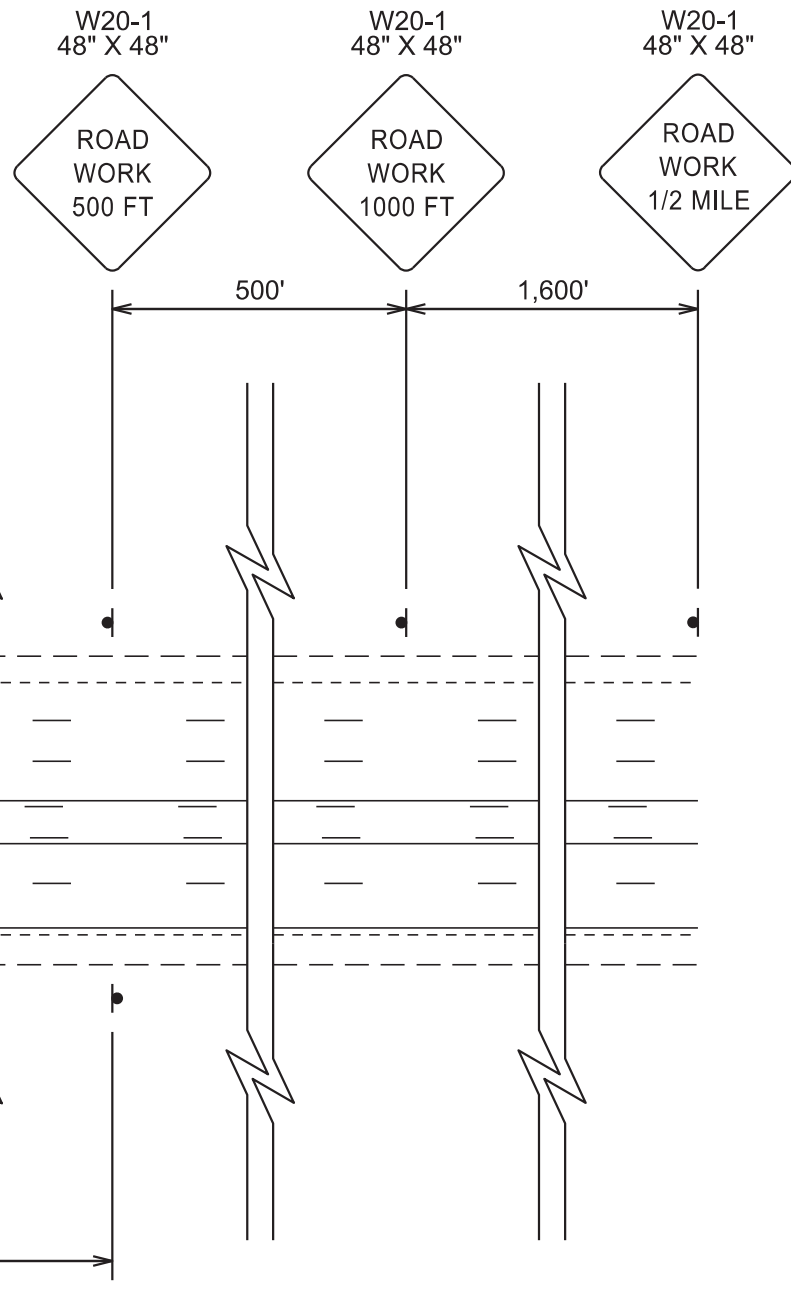
BEGIN PROJ. NO. HSIP-43(47)
27004-3284-94 (CONST.)
STA. 448+42.79 S.R. 43
N: 583561.2849
E: 1153665.7987

BEGIN PROJ. NO. HSIP-43(47)
27004-2284-94 (R.O.W.)
STA. 450+25.10 S.R. 43
N: 583742.8249
E: 1153649.0580

S.R. 43 STA. 452+50.00 =
KEFAUVER DR. STA. 5+00.00
N:583966.7512
E:1153628.2057

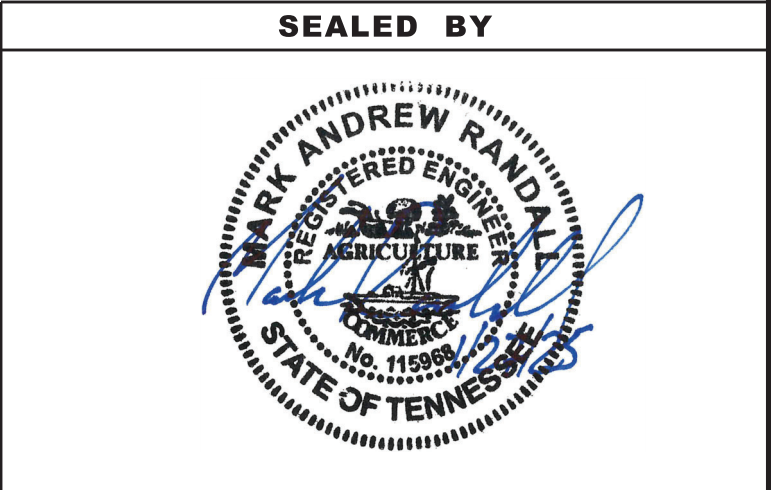
END PROJ. NO. HSIP-43(47)
27004-3284-94 (CONST.)
STA. 457+60.02 S.R. 43
N: 584474.6250
E: 1153581.5023

END PROJ. NO. HSIP-43(47)
27004-2284-94 (R.O.W.)
STA. 454+12.00 S.R. 43
N: 584128.0485
E: 1153613.1364



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE
	FLEXIBLE DRUMS (CHANNELIZING)
	SIGN (CONSTRUCTION)
	TRAFFIC FLOW

PHASE I



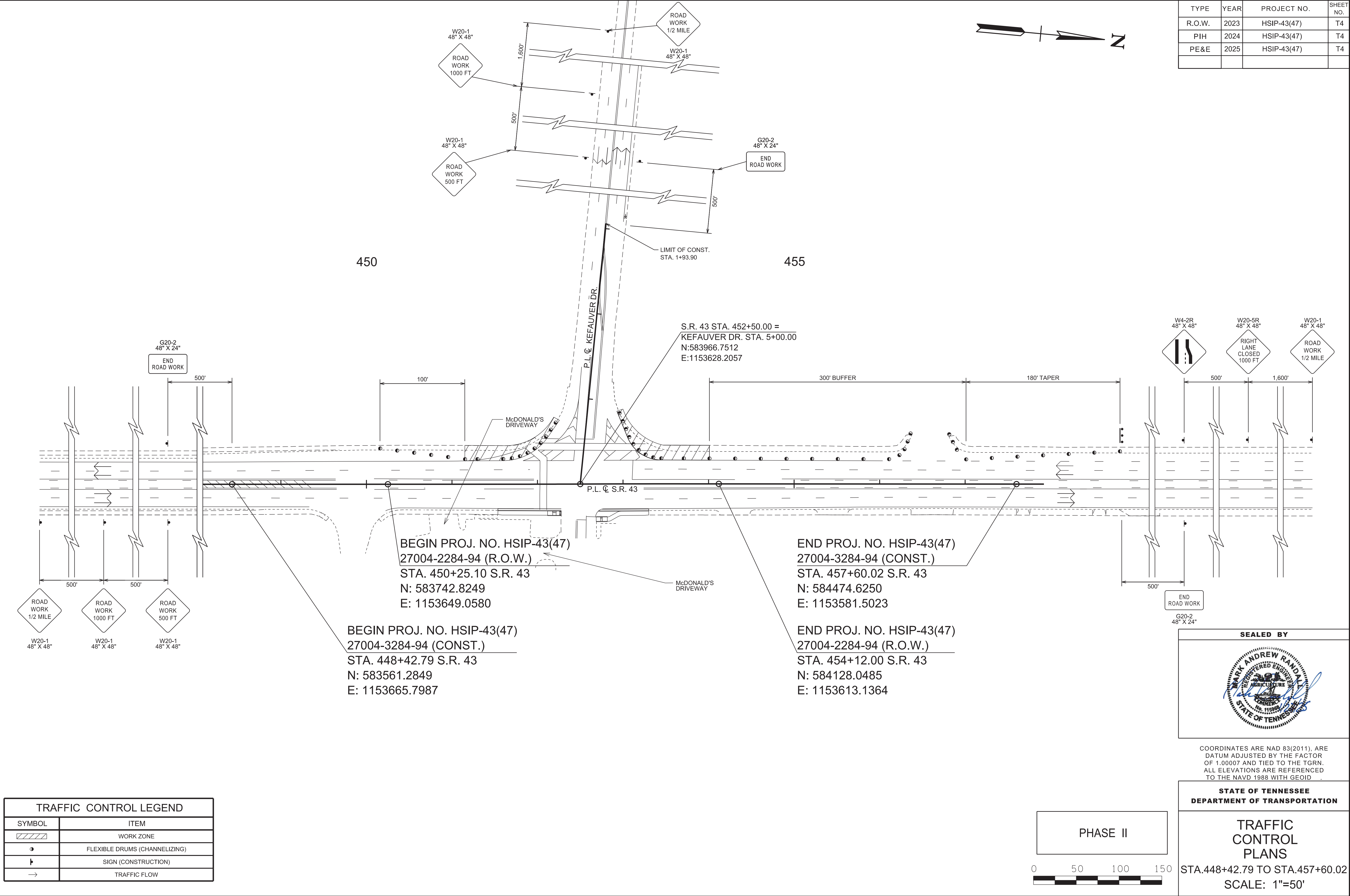
COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
STA.448+42.79 TO STA.457+60.02
SCALE: 1"=50'

1/6/2025
P:\2020\200145\SalInfrastructure\Cad\Dgn\T4.sht

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	T4
PIH	2024	HSIP-43(47)	T4
PE&E	2025	HSIP-43(47)	T4



450

455

W20-1
48" X 48"
ROAD WORK
1000 FT

W20-1
48" X 48"
ROAD WORK
500 FT

ROAD WORK
1/2 MILE

W20-1
48" X 48"

G20-2
48" X 24"
END
ROAD WORK

LIMIT OF CONST.
STA. 1+93.90

S.R. 43 STA. 452+50.00 =
KEFAUVER DR. STA. 5+00.00
N:583966.7512
E:1153628.2057

G20-2
48" X 24"
END
ROAD WORK

BEGIN PROJ. NO. HSIP-43(47)
27004-2284-94 (R.O.W.)
STA. 450+25.10 S.R. 43
N: 583742.8249
E: 1153649.0580

BEGIN PROJ. NO. HSIP-43(47)
27004-3284-94 (CONST.)
STA. 448+42.79 S.R. 43
N: 583561.2849
E: 1153665.7987

END PROJ. NO. HSIP-43(47)
27004-3284-94 (CONST.)
STA. 457+60.02 S.R. 43
N: 584474.6250
E: 1153581.5023

END PROJ. NO. HSIP-43(47)
27004-2284-94 (R.O.W.)
STA. 454+12.00 S.R. 43
N: 584128.0485
E: 1153613.1364

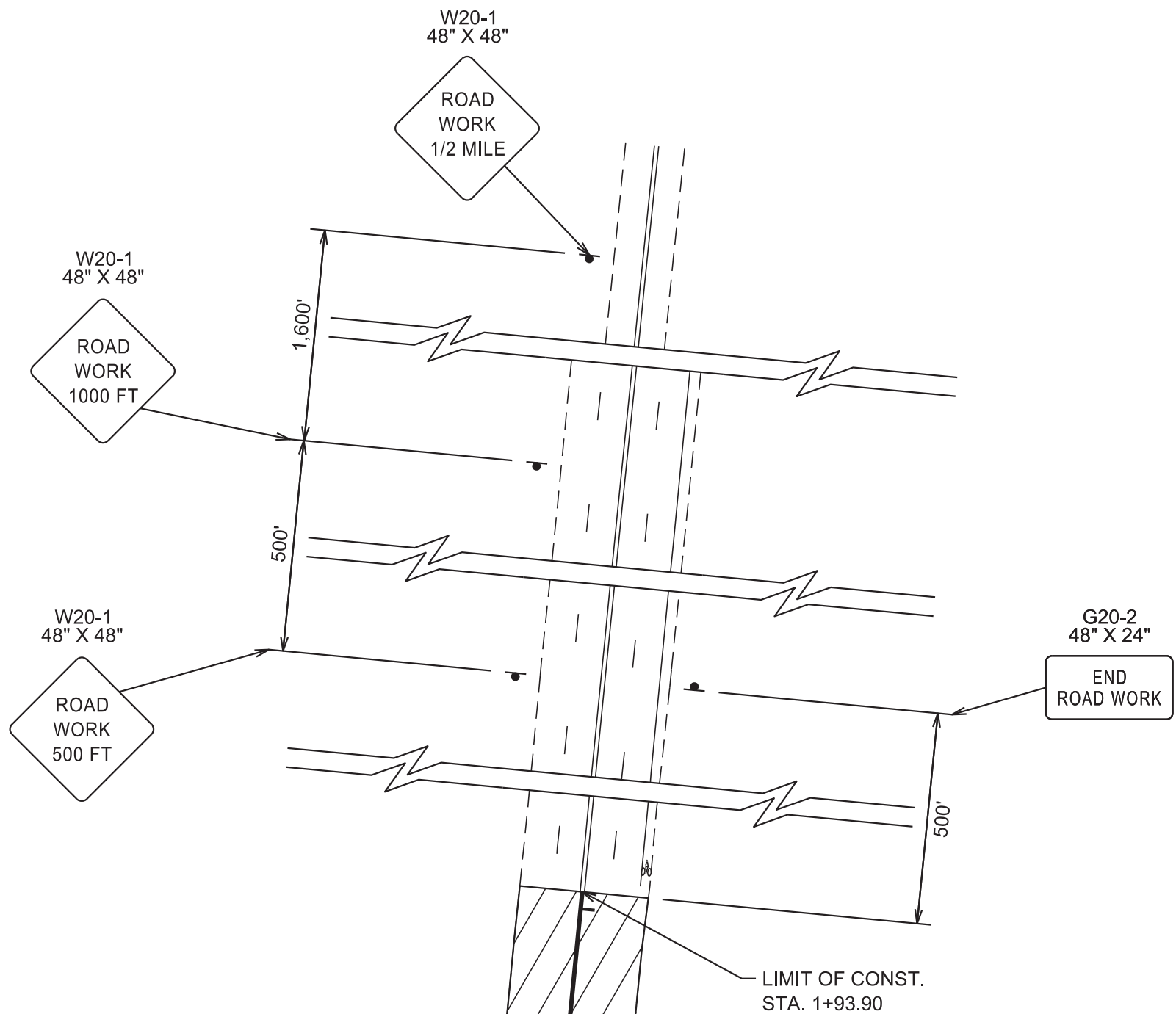
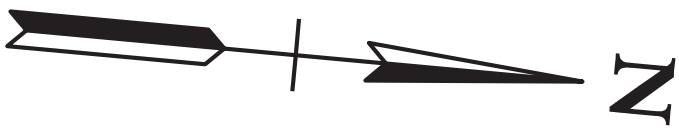
W4-2R
48" X 48"
ROAD WORK
1/2 MILE

W20-5R
48" X 48"
RIGHT
LANE
CLOSED
1000 FT

W20-1
48" X 48"
ROAD WORK
1/2 MILE

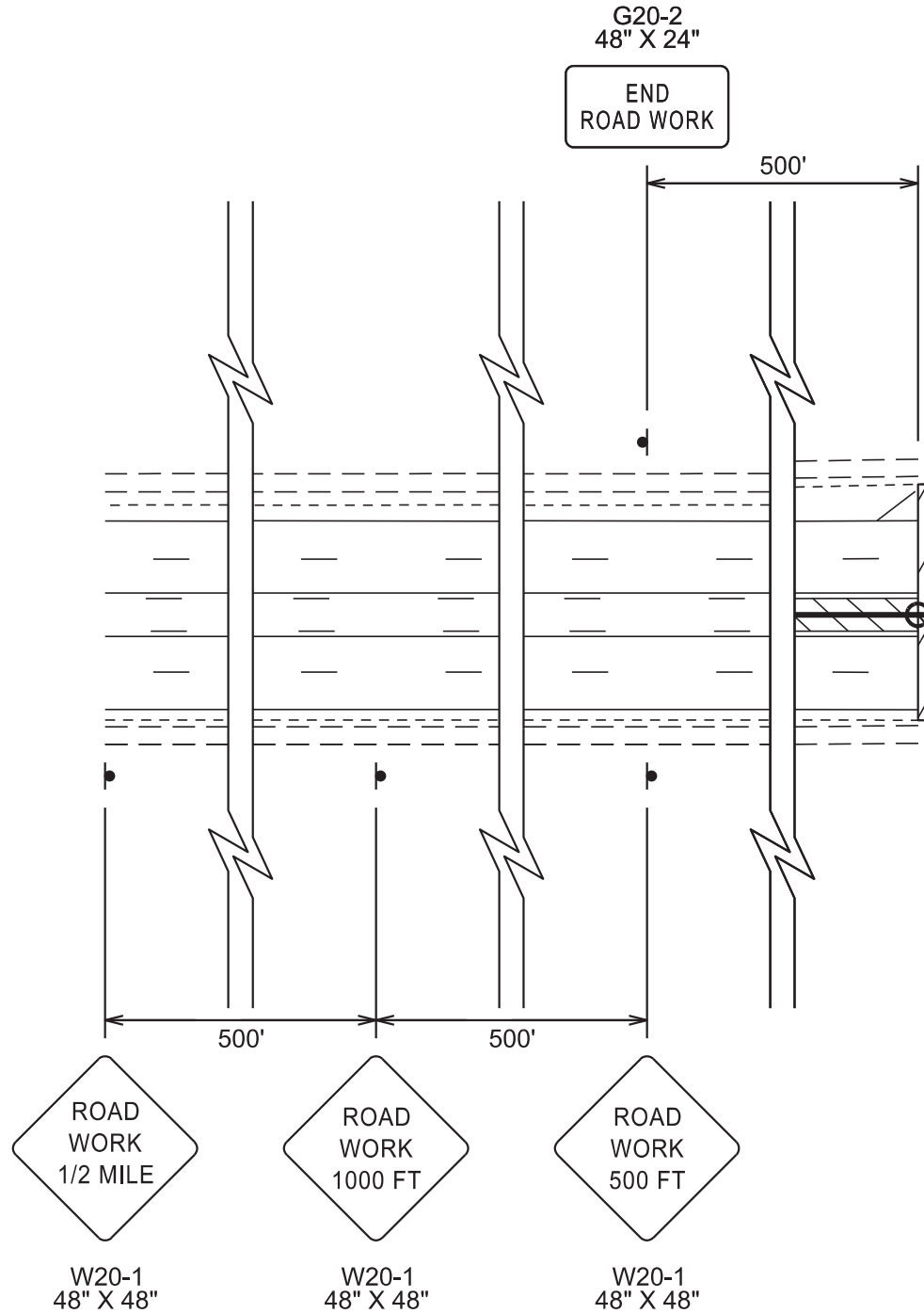
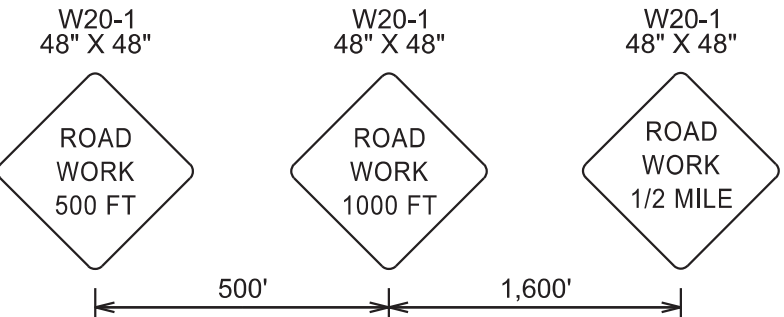
END
ROAD WORK
G20-2
48" X 24"

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2023	HSIP-43(47)	T5
PIH	2024	HSIP-43(47)	T5
PS&E	2025	HSIP-43(47)	T5



BEGIN PROJ. NO. HSIP-43(47)
27004-2284-94 (R.O.W.)
STA. 450+25.10 S.R. 43
N: 583742.8249
E: 1153649.0580

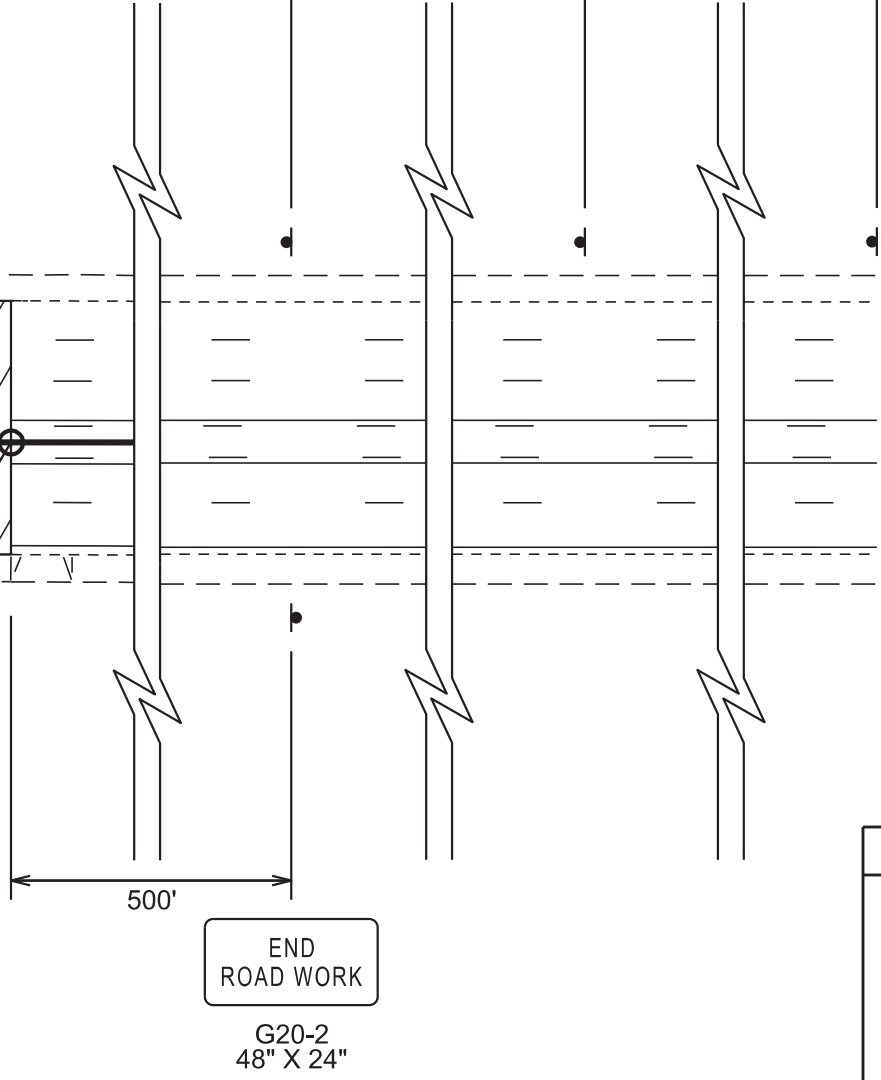
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KEFAUVER DR. STA. 5+00.00
N:583966.7512
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BEGIN PROJ. NO. HSIP-43(47)
27004-3284-94 (CONST.)
STA. 448+42.79 S.R. 43
N: 583561.2849
E: 1153665.7987

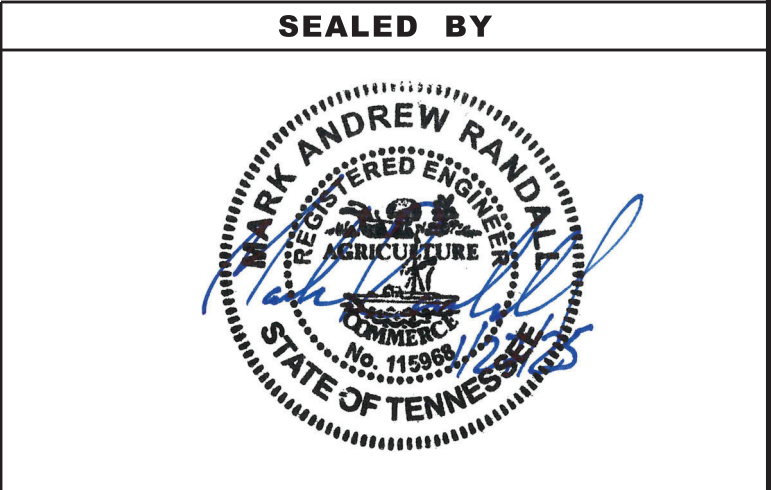
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STA. 457+60.02 S.R. 43
N: 584474.6250
E: 1153581.5023

END PROJ. NO. HSIP-43(47)
27004-2284-94 (R.O.W.)
STA. 454+12.00 S.R. 43
N: 584128.0485
E: 1153613.1364



TRAFFIC CONTROL LEGEND	
SYMBOL	ITEM
	WORK ZONE

PHASE III



COORDINATES ARE NAD 83(2011), ARE DATUM ADJUSTED BY THE FACTOR OF 1.00007 AND TIED TO THE TGRN. ALL ELEVATIONS ARE REFERENCED TO THE NAVD 1988 WITH GEOID

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TRAFFIC
CONTROL
PLANS
STA.448+42.79 TO STA.457+60.02
SCALE: 1"=50'

17-DEC-2024 09:24 \\AG03SDCW\F00008.net.ads.state.tn.us\13\SHARED\UTILITY\REGION 4\UTILITY PROJECTS\COUNTIES\GIBSON\129810.00\129810.00-Gibson Co. SR-43.dgn

Index Of Sheets	
SHEET NAME	SHEET NO.
UTILITIES INDEX, UTILITIES OWNERS, AND UTILITIES SHEETS;	U1-1
PIN: 129810.00	

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

BUREAU OF ENGINEERING

GIBSON COUNTY

SR - 43

(US-45E, SOUTH FIRST STREET) INTERSECTION AT KEFAUVER DRIVE, LM 8.70 IN MILAN

STATE HIGHWAY NO. 43 F.A.H.S. NO. 45E

CONTRACT TYPE	UTILITY	UTILITY OWNERS & CONTACTS:	CONTRACT TYPE	UTILITY	UTILITY OWNERS & CONTACTS:
NO COST	TELEPHONE FIBER OPTIC	AT&T DANIEL POTTS 315 E. COLLEGE STREET JACKSON, TN. 38301 901-488-2359 dp7607@att.com	NO CONFLICT	GAS	GIBSON COUNTY UTILITY DISTRICT JEREMY BUTLER 1300 N. HWY. 45 TRENTON, TN. 38382 731-855-1441 jeremy@gcud.net
NO CONFLICT	CABLE	HUGHES NET JOHN FANELLI 11717 EXPLORATION LANE GERMANTOWN, MD 20876 301-482-7124 john.fanelli@hughes.com			
NO COST	CATV	CHARTER COMMUNICATIONS KEITH CHESSER 24 CIRCLE DRIVE MCKENZIE, TN 38201 731-352-1146 / C: 731-621-9552 keith.chesser@charter.com			
NO CONFLICT	WATER SEWER	MILAN DEPT. OF PUBLIC UTILITIES / WORKS DAN HUMPHREYS 1085 S 2ND STREET / P. O. BOX 109 MILAN, TN 38358 731-686-1537 / Cell: 731-414-7545 dhumphreys@mpu1.com			
PRIOR MOVE	ELECTRIC	MILAN DEPT. OF PUBLIC UTILITIES / WORKS JIM SUTCLIFFE 1085 S 2ND STREET / P. O. BOX 109 MILAN, TN 38358 731-686-1537 / Cell: 731-414-8125 jsutcliffe@mpu1.com			

SPECIAL NOTES

SOME UTILITIES CAN BE LOCATED BY CALLING THE TENNESSEE ONE SYSTEM, INC. AT 1-800-351-1111.

TYPE	YEAR	PROJECT NO.	SHEET NO.
PS&E	2025	HSIP-43(47)	U1-1

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UTILITY INDEX
AND
UTILITY OWNERS